

TOWNSHIP OF NEPTUNE TRANSIT VILLAGE STUDY AREA LAND USE PLAN



Prepared for the Township of Neptune by T&M Associates with financial support from the Office of Smart Growth of the New Jersey Department of Community Affairs

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NEPTUNE TOWNSHIP TRANSIT VILLAGE STUDY AREA LAND USE PLAN

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Table of Contents

Introduction	1
Purpose	2
Process	3
Neptune Strategic Revitalization Plan	3
Smart Future Grant	4
Technical Report	4
Public Outreach	5
Neptune Township Transit Village Study Area Land Use Plan	6
Form-Based Codes	7
Goals and Objectives	9
Land Use Plan	10
Commercial Zones	10
Residential Zones	12
Core Design Concepts	13
An Attractive Streetscape	13
Pedestrian-Oriented Development	14
Improve Pedestrian Linkages to the Railroad Station	15
Provide a Mix of Uses	16
Form-Based Code	17
Building Placement	17
Building Form Requirements	22

Table of Contents (Continued)

Public Frontages and Thoroughfare Assemblies	34
Memorial Drive	34
5 th , 6 th , 7 th , 8 th , and 9 th Avenues	39
Atkins Avenue	42
Traffic Calming	45
Thoroughfare Assemblies	48
Parking and Signage	48

APPENDIX

A: Background Study and Technical Report	51
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Introduction

This Plan provides the necessary framework to implement the rezoning of a neighborhood in the Bradley Park section of Neptune Township. The neighborhood, which contains a mixture of industrial, commercial and residential properties, is located in close proximity to the Bradley Beach railroad station. Neptune has retained T&M Associates to review the neighborhood characteristics with a view toward possible rezoning concepts for transit-oriented uses.

The initial concept consisted of a transit village plan based on a mixed-use development. Mixed-use development incorporates a variety of land uses into a building or a set of buildings. This type of development is generally located along transit nodes in order to promote public transit ridership and walkability, and in turn, decrease automobile dependence.

This Plan creates a vision for this neighborhood based on community input and the goals of the Neptune Strategic Revitalization Plan.

Purpose

The purpose of the Neptune Township Transit Village Study Area Land Use Plan is to establish a comprehensive set of guidelines that are intended to guide the form of future development within the Study Area. Specifically, a form-based code has been developed for the Study Area in order to ensure that future development takes place at an appropriate scale, and takes into consideration how development projects will impact the aesthetics and function of the public realm. In addition, the form-based code for the Study Area establishes design standards for public spaces such as streets and sidewalks, as well as building form standards that are intended to foster harmony among the various elements of the built environment. By governing the form of buildings and public spaces, it will be possible to create a built environment that is aesthetically pleasing and meets all of the functional needs of its users.

Process

Neptune Strategic Revitalization Plan

In 2004, Neptune Township adopted the Neptune Strategic Revitalization Plan (NSRP), a plan that provides a comprehensive strategy for the revitalization of Neptune Township, and in particular, the Midtown, Bradley Park, and Shark River Waterfront neighborhoods. This plan sets forth specific goals and objectives for the Township's revitalization efforts. These goals and objectives, as they apply to the Neptune Township Transit Village Study Area, are to:

- Preserve and enhance the existing character of the residential neighborhoods in Midtown, Bradley Park and Shark River areas while providing a mix of housing types and uses;
- Reclaim underutilized and/or constrained parcels for productive use;
- Provide a circulation network in the Midtown, Bradley Park, and Shark River areas for vehicles, bicycles and pedestrians that permits safe and efficient travel; and
- Coordinate revitalization efforts with public transportation enhancements, including new public transportation opportunities in the Township.

In addition to establishing goals for the Study Area, the NSRP establishes an action plan that is designed to provide a framework for creating a transit village plan for the area. These actions include:

- Preparing and adopting a transit village plan for the area;
- Providing a mix of uses, including retail commercial and personal service uses that would meet the needs of residents of the transit village, Bradley Beach, and attract commuters to Neptune;
- Install pedestrian improvements at Memorial Drive to improve pedestrian access to Bradley Beach Train Station; and

- Improve sidewalks and enhance streetscapes along 5th, 6th, 7th, 8th and, 9th Avenue corridors to improve pedestrian linkages between the existing Bradley Park residential neighborhood and the Bradley Beach Train Station.

The Neptune Township Transit Village Plan is a step toward achieving the goals and implementing the actions set forth in the NSRP.

Smart Future Grant

In order to complete the Transit Village Plan, the Township of Neptune applied for, and was awarded, a \$40,000 Smart Future Planning Grant from the Office of Smart Growth of the New Jersey Department of Community Affairs. Smart Future Planning Grants are part of the Smart Future Initiative, which promotes smart growth planning between municipalities and counties throughout New Jersey.¹

Technical Report

Prior to the creation of the Transit Village Study Area Land Use Plan, the Township retained T&M Associates to produce a technical report documenting baseline conditions within the Study Area. T&M Associates completed the Background Study and Technical Report of the Neptune Transit Village Study Area on August 7, 2007. This report documents existing land uses, homeownership trends, transportation network conditions, and land use and zoning regulations governing the Study Area, as well as policies contained in the Neptune Township Master Plan, State, County, and surrounding municipal plans. The complete Background Study and Technical Report can be found in Appendix A of this document.

¹ <http://www.state.nj.us/dca/news/news/2007/approved/070614a.html>

Public Outreach

T&M Associates, in conjunction with Neptune Township, held a series of two community visioning workshops in an effort to solicit community input during the planning process. The first of these meetings was held on January 29, 2008 and intended to develop a vision statement, prepare a conceptual land use plan, and examine acceptable building forms, scale, and relationships with the Study Area. The second meeting was held on March 26, 2008 and allowed residents to take a “hands on” approach to placing various building types to “build out” the Study Area. Public input from each of these meetings was carefully considered in the creation of the Transit Village plan for the Study Area.

The public involvement process generated important feedback regarding the future of the Neptune Township Transit Village Study Area. Area residents suggested that the Study Area should:

- Include a mix of moderate and low income housing of a mixed bedroom count for sale and rent;
- Incorporate Front yard space and porches for newly constructed buildings;
- Incorporate eco-friendly technologies in any new construction;
- Contain new development that is designed to maintain the existing fabric and character of the community;
- Include locally-owned businesses;
- Have commercial uses concentrated along Memorial Drive to accommodate commuters and locals;
- Concentrate new development in the northern portions of the Study Area;
- Incorporate design mechanisms that create safe, complete, easily traversed streets that are accessible to all users;
- Incorporate boulevard treatments, such as planted medians, along Memorial Drive
- Include a safe crossing of Memorial Drive for easy access to the Bradley Beach Train Station.

Area residents expressed that they do not want:

- Buildings that exceed four (4) stories in height within the Study Area;
- Existing businesses to be forced out by zoning changes;
- Eminent domain to be used to acquire properties within the Study Area;
- Any changes in zoning to occur without first notifying area residents of the proposed changes.

Neptune Township Transit Village Study Area Land Use Plan

The Neptune Township Transit Village Study Area Land Use Plan is intended to govern the use and form of the Neptune Township Transit Village Study Area. It is designed to facilitate the development of a relatively dense, mixed-use community focused around the Bradley Beach Train Station that contains a variety of land uses and housing types. The plan takes into account the public comments solicited during the series of two community visioning workshops, as well as the existing character of the Study Area.

The remainder of the document establishes goals and objectives, a land use plan, and a form-based code for the Study Area.

Form-Based Codes²

Form-based codes are a method of regulating development to achieve a specific form. Form-based codes create a predictable public realm primarily by controlling physical form, with a lesser focus on land use, through municipal or county regulations.

Form-based codes address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes, presented in both diagrams and words, are keyed to a regulating plan that designates the appropriate form and scale, and, therefore, character of development, rather than only distinctions in land use types.

Form-based codes commonly include the following elements

- *Regulating Plan*: A plan or map of the regulated area designating the locations where different building form standards apply, based on clear community intentions regarding the physical character of the area being coded;
- *Public Space Standards*: Specifications for the elements within the public realm (e.g., sidewalks, travel lanes, on-street parking, street trees, street furniture, etc.);
- *Building Form Standards*: Regulations controlling the configuration, features, and functions of buildings that define and shape the public realm;
- *Administration*: A clearly defined application and project review process;
- *Definitions*: A glossary to ensure the precise use of technical terms.

² For additional information see the website of the Form-Based Codes Institute at www.formbasedcodes.org

Form-based codes also may include:

- *Architectural Standards*: Regulations controlling the external architectural materials and quality;
- *Landscaping Standards*: Regulations controlling landscape design and plant materials on private property as they impact public spaces (e.g. regulations about parking lot screening and shading, maintaining sight lines, insuring unobstructed pedestrian movements, etc.);
- *Signage Standards*: Regulations controlling allowable signage sizes, materials, illumination, and placement;
- *Environmental Resource Standards*: Regulations controlling issues such as storm water drainage and infiltration, development on slopes, tree protection, solar access, etc.;
- *Annotation*: Text and illustrations explaining the intentions of specific code provisions.

Goals and Objectives

The goals and objectives of the Neptune Township Transit Village Study Area Land Use Plan are to:

- Maximize the locational advantage of the neighborhood to the Bradley Beach Train Station by creating residential housing opportunities;
- Minimize land use conflicts between the industrial, commercial, and residential land uses; and,
- Build a “sense of place³” with good urban design through controls on building massing, streetscape improvements roadway improvements and building design guidelines.

³ According to the National Trust for Historic Preservation, “sense of place” can be defined as “Those things that add up to a feeling that a community is a special place, distinct from anywhere else.” (<http://envstudies.brown.edu/oldsite/Thesis/2001/james/senseofplace.html>)

Land Use Plan

The Neptune Township Transit Village Study Area is currently zoned for industrial land uses. In order to foster a walkable, transit-oriented district that contains a variety of land uses, a Transit Village Overlay District will be created within the Study Area. The following section outlines the categories that will be implemented as part of the Transit Village Overlay District to achieve the vision of the Neptune Township Transit Village Study Area Land Use Plan.

Commercial Zones

The Study Area contains several viable commercial businesses including restaurants, produce stands, and retail establishments. Commercial establishments are vital to creating and sustaining mixed-use communities, because they are used by residents and visitors alike. Commercial uses are of particular importance to the Neptune Township Transit Village Study Area because of the Study Area's proximity to the Bradley Beach Train Station. Retail establishments such as restaurants, convenience stores, and service establishments such as dry cleaners and pharmacies will serve not only neighborhood residents, but also commuters, as well as vacationers during the summer months.

During the Community Visioning Workshop that was held on January 29, 2008, several residents suggested that commercial uses should be concentrated along Memorial Drive in order to accommodate locals, commuters, and visitors alike. During the Community Visioning Workshop, residents suggested the following commercial uses:



**Figure 1: Mom's Kitchen, located at the corner of 5th Avenue and Atkins Avenue. Restaurants and other commercial establishments will be integral to the creation of a mixed-use community within the Study Area.
Source: T&M Associates**

- Laundromat
- Dry Cleaners
- Convenience store with DVD rental
- Children’s entertainment (e.g., internet café)
- Coffee shop
- Diner
- Sandwich shop
- Pizzeria

To provide essential commercial uses to the Study Area, the following lots shall be designated as commercial lots within the Transit Village Overlay District:

Block 162	Lot 79	Block 163	Lot 66	Block 164	Lot 52	Block 165	Lot 36
	Lot 78		Lot 65		Lot 51		Lot 35
	Lot 77		Lot 55		Lot 50		Lot 34
	Lot 69		Lot 54		Lot 39		Lot 13
	Lot 68		Lot 53		Lot 38		
	Lot 67		Lot 64		Lot 37		

In addition to the aforementioned lots, any lot within the Study Area on which a commercial use already exists shall also be designated as a commercial lot within the Transit Village Overlay District.

Residential Zones

Residential uses occupy approximately 18.7 percent of the total land contained within the Study Area. Approximately 55 housing units are spread across 2.67 acres of land within the Study Area. The Study Area contains a wide variety of housing types, including single-family detached homes, townhouses, apartments, and mobile homes. Preserving the existing housing stock within the Study Area, as well as fostering the construction of new housing for potential new residents is an important part of creating and maintaining a viable mixed-use, transit-oriented neighborhood.



**Figure 2: A cluster of single-family detached homes on 7th Avenue. The Study Area contains a substantial number of housing units and a variety of housing types.
Source: T&M Associates**

The following lots shall be designated as residential lots within the Transit Village Overlay District:

Block 162		Block 163		Block 164		Block 165	
Lot 1	Lot 71	Lot 56	Lot 67	Lot 1	Lot 10	Lot 14	Lot 26
Lot 2	Lot 72	Lot 57	Lot 69	Lot 2	Lot 40	Lot 16	Lot 27
Lot 9	Lot 73	Lot 59	Lot 70	Lot 3	Lot 42	Lot 17	Lot 28
Lot 10	Lot 74	Lot 60	Lot 71	Lot 4	Lot 44	Lot 18	Lot 29
Lot 11	Lot 75	Lot 61	Lot 72	Lot 5	Lot 45	Lot 19	Lot 30
Lot 12	Lot 76	Lot 62	Lot 73	Lot 6	Lot 46	Lot 20	Lot 31
Lot 70		Lot 63	Lot 74	Lot 7	Lot 47	Lot 23	Lot 32
				Lot 8	Lot 48	Lot 24.01	Lot 33
				Lot 9	Lot 49	Lot 25	

Core Design Concepts

The core design concepts for the Neptune Township Transit Village Study Area seek to create a viable mixed-use, walkable community within the Study Area. The design concepts pay particular attention to the way users will interact with the physical environment, and the functional needs and desires of visitors and residents. The core design concepts are derived from public comments and concerns that were expressed at a series of Community Visioning Workshops.

An Attractive Streetscape

Streetscapes play a major role in determining the overall physical appearance of a community and how a community will be navigated by its users. Safe, attractive streetscapes have the potential to increase the number of pedestrian visitors to the area, and may encourage traditional auto users to park once and move through the district as a pedestrian. Attractive Streetscapes are integral parts to fostering a pedestrian-oriented community.

During the series of Community Visioning Workshops, community members suggested a number of improvements that should be made to existing streets within the Study Area. These suggested improvements included:

- Beautifying the “strip” between Memorial Drive and the railroad track with shrubs, trees, other vegetation, and a community amenity such as a jogging trail;
- Improving street lighting by installing new streetlights and replacing burnt-out bulbs;
- Constructing ADA-compliant (Americans with Disabilities Act) sidewalk improvements;
- Planting street trees throughout the Study Area;
- Providing curb ramps throughout the Study Area;
- Repairing broken curbs and sidewalks;
- Providing bike lanes throughout the Study Area;

- Maintaining streets with regular street cleaning;
- Providing underground utilities;
- Constructing a planted traffic median on Memorial Drive
- Improving lighting along Memorial Drive
- Properly delineating crosswalks to make them visually apparent
- Providing community signage (e.g. “Welcome to Bradley Park”) at key entrance points
- Using sidewalk pavers or stamping at corners and other key locations

By implementing the suggestions outlined above, streetscapes within the Study Area will become increasingly attractive, safe, and tailored to the needs of their users.

Pedestrian-Oriented Development

A key component of transit-oriented development is walkability. Local residents who live within close proximity to the Bradley Beach Train Station should have the ability to walk to the train station along safe, attractive streets that are lined with a variety of business and housing types.

Two important elements of successful pedestrian-oriented developments are pedestrian safety and appropriately-scaled development. Pedestrian safety, including buffering from vehicular traffic, wide sidewalks, adequate lighting, and dedicated bike lanes, encourages those who might not otherwise walk to, or around, an area to consider doing so, thereby potentially increasing pedestrian usage of an area. Adequate pedestrian safety mechanisms benefit not only public safety, but also public health. Encouraging pedestrian usage of an area has the potential to reduce vehicle emissions, and increase the health and fitness of residents and visitors alike.

The Community Visioning Workshop attendees made the following suggestions for increasing safety within the Study Area:

- Increase visibility at intersections;
- Construct a pedestrian bridge at Sixth Avenue or Seventh Avenue (or between the pair) and Memorial Drive;
- Install traffic signals at the intersection of Memorial Drive and Seventh Avenue as an alternative to construction a pedestrian bridge over Memorial Drive;
- Make all community features ADA-compliant, where applicable;
- Lower speed limits district wide;
- Improve lighting along Memorial Drive;
- Increase police presence within the area;
- Construct fencing along the train tracks to eliminate pedestrian crossing at non-designated points.

In addition to increasing the safety of pedestrians within the Study Area, it is also important to appropriately scale development in order to allow pedestrians to access a variety of uses on foot. Specifically, the form-based code that follows this section seeks to regulate development in a manner that promotes attractive, yet dense, development with a mix of uses and a variety of amenities for pedestrians.

Improve Pedestrian Linkages to the Railroad Station

The foundation of a transit-oriented development is its proximity to public transit, especially rail stations. The Study Area is located adjacent to the Bradley Beach Train Station, which is a stop on New Jersey Transit's North Jersey Coast Line. Although the Study Area is adjacent to the Bradley Beach Train Station, pedestrian linkages from the Study Area to the Station are limited or hazardous. The following form-based code establishes a set of guidelines that will regulate the linkages to the Bradley Beach Train Station in order to ensure that the station is accessible by safe, pedestrian-oriented routes.

Provide a Mix of Uses

Providing a mix of uses within the Study Area is an important part of facilitating pedestrian movement within the area. By encouraging development that includes a variety of uses, residents and visitors are more likely to find the amenities they need within walking distance from their homes or parking areas, thereby lessening vehicular traffic within the area.

Form-Based Code

The following sections contain the proposed form-based code for the Study Area. The elements that constitute the form-based code are rooted in the information contained in the previous sections of the plan, as well as public comments and concerns that were expressed at the Community Visioning Workshops. The regulations set forth in the following sections are intended to preserve the existing assets of the Study Area and guide the form of future development within the Study Area while paying particular attention to the form of the built environment, and how users will interact with it.

In order to implement a form-based code that is consistent with the vision established during the series of Community Visioning Workshops, the parcels contained in Neptune Township Transit Village Study Area have been divided into three distinct areas: Form Area A (Lots with frontage on Memorial Drive, including corner lots), Form Area B (Lots with frontage on 5th, 6th, 7th, 8th, and 9th Avenues, excluding corner lots), and Form Area C (Lots with frontage on Atkins Avenue, including corner lots). Each of these classes is governed by a set of design guidelines that have been tailored to achieve the desired vision for the area.

1. Building Placement

Building placement refers to the orientation and location of a building on a given lot. Building placement within the Study Area is governed by the following setback requirements:

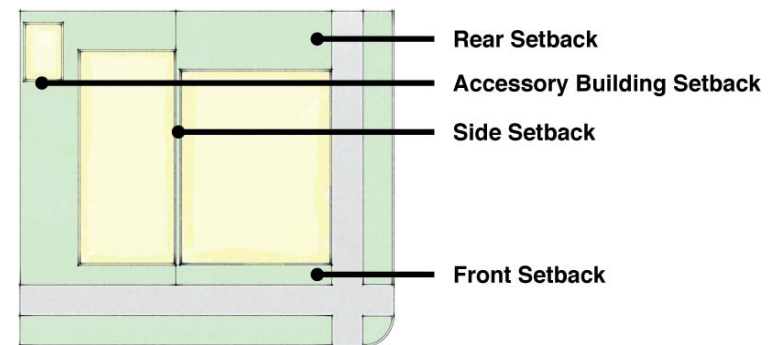


Figure 3: Diagram of building setback elements
(Source: T&M Associates)

A. Form Area A

The setback requirements that govern the Form Area A are intended to promote a dense, mixed-use thoroughfare along Memorial Drive. Commercial development within the Study Area shall be concentrated along the Memorial Drive Corridor. In addition, the Memorial Drive Corridor shall allow for the construction of mixed-use buildings that contain commercial as well as residential uses. The following setback requirements govern development on lots that have frontage on Memorial Drive, including corner lots:

1) Principal Building Setbacks

- Front Yard Setback: 5 feet minimum, 20 feet maximum
- Side Yard Setback: 0 feet minimum, 15 feet maximum
- Rear Yard Setback: 0 feet minimum, no maximum

2) Accessory Building Setbacks

- Front Yard Setback: 24 feet minimum, plus principal building setback
- Side Yard Setback: 0 feet minimum, no maximum
- Rear Yard Setback: 3 feet minimum, no maximum

3) Lot Width and Coverage

- Lot Width: 40 feet minimum, 80 feet maximum
- Lot Coverage: 70 percent maximum

4) Density/Intensity Standards

- Maximum Residential Density: 8 dwelling units/acre



Figure 4: Building setback requirements in Form Area C (The Memorial Drive Corridor) are designed to allow for the construction of dense, mixed-use buildings. (Photo Source: The Villages at Belvoir) <https://belvoir.pinnaclefamilyhousing.com/latestnews/news082306.asp>

5) Building Configuration

- Principal Building Height: 48 feet
- Principal Building Height: 4 stories maximum, 2 stories minimum
- Accessory Building Height: 25 feet
- Accessory Building Height: 2 stories maximum

B. Form Area B

The building setback requirements that govern Form Area B (5th, 6th, 7th, 8th and 9th Avenues) are intended to maintain the existing residential character of the Study Area. The following setback requirements govern development on lots within Form Area B:

1) Principal Building Setbacks

- Front Yard Setback: 12 feet minimum, 25 feet maximum
- Side Yard Setback: 12 feet minimum, no maximum
- Rear Yard Setback: 12 feet minimum, no maximum

2) Accessory Building Setbacks

- Front Yard Setback: 20 feet minimum, plus principal building setback
- Side Yard Setback: 6 feet minimum, no maximum
- Rear Yard Setback: 6 feet minimum, no maximum

3) Lot Width and Coverage

- Lot Width: 50 feet minimum, 100 feet maximum
- Lot Coverage: 60 percent maximum

4) Density/Intensity Standards



Figure 5: The established setback requirements for Form Area B are intended to preserve the existing residential character of the neighborhood within the Study Area (Photo Source: T&M Associates)

- Maximum Residential Density: 6 dwelling units/acre

5) Building Configuration

- Principal Building Height: 36 feet
- Principal Building Height: 3 stories maximum
- Accessory Building Height: 25 feet
- Accessory Building Height: 2 stories maximum

C. Form Area C

The building setback requirements that govern lots within Form Area C (Atkins Avenue) are intended to preserve the existing residential character of the Transit Village Study Area. While the setback requirements listed below do not permit an increase in residential density as compared to the established setbacks for Form Area B, the reduced setback requirements allow for larger buildings, thereby giving the impression of an increasingly dense residential district. The following design guidelines govern lots within Form Area A:

1) Principal Building Setbacks

- Front Yard Setback: 10 feet minimum, 20 feet maximum
- Side Yard Setback: 8 feet minimum, no maximum
- Rear Yard Setback: 10 feet minimum, no maximum

2) Accessory Building Setbacks

- Front Yard Setback: 18 feet minimum, plus principal building setback
- Side Yard Setback: 6 feet minimum, no maximum
- Rear Yard Setback: 6 feet minimum, no maximum

3) Lot Width and Coverage

- Lot Width: 40 feet minimum, 80 feet maximum
- Lot Coverage: 65 percent maximum

4) Density/Intensity Standards

- Maximum Residential Density: 6 dwelling units/acre

5) Building Configuration

- Principal Building Height: 36 feet
- Principal Building Height: 3 stories maximum
- Accessory Building Height: 25 feet
- Accessory Building Height: 2 stories maximum

2. Building Form Requirements

The following section regulates the physical form and architectural elements of the buildings within the Study Area. The design guidelines below are intended to foster a “sense of place”, or a unique and distinct physical character, within the Study Area.

The Study Area initially developed as an industrial area that was centered around a freight railroad line. Today, the Study Area contains a variety of industrial, commercial, and residential uses, and distinct architectural styles. Perhaps the most iconic building within the Study Area is the “Monmouth Piano” Building, a factory building dating from the 1890s that is currently being used as a retail establishment that specializes in piano sales. In addition, the housing stock within the Study Area is comprised of a variety of architectural styles including historic styles such as the Bungalow, the Dutch Colonial, and the Folk Victorian, as well as more contemporary home types such as townhouses and mobile homes.



Figure 6: The Monmouth Piano Building, an 1890s factory building that has been refurbished and houses Monmouth Piano, is perhaps the most iconic building within the Study Area (Source: T&M Associates)

The building form standards below are rooted in the existing architectural character of the Study Area and seek to promote development within the Study Area that enhances its “sense of place”.

A. General Requirements for Residential Buildings

Newly constructed buildings and buildings undergoing restoration within the Study Area shall be constructed or restored in an architectural style that is representative of the existing character of the area. Oversized single-family dwellings of any design style shall be prohibited. In addition, mobile homes, log cabins, summer bungalows, and residential design solutions resembling regional styles such as “southwestern”, or “Old World English Tudor” that conflict with the heritage and composition of the Transit Village Study Area and the Bradley Park Neighborhood shall be prohibited.

Residential buildings shall be constructed or restored in one of the following architectural styles, or another architectural style that is deemed to be consistent with the character of the Study Area:

1. Folk Victorian⁴

Folk Victorian homes are common within the Study Area and throughout Monmouth County, especially in historic neighborhoods. Folk Victorian homes are characterized by their square, symmetrical shape, steeply pitched roofs, overhangs, and porches. Folk Victorians are typically constructed of a mix of wood and masonry materials, and contain three stories—two main floors, and an attic.



Figure 7 : Folk Victorian homes are common within the Study Area and the surrounding areas.

(Source:
<http://images3.zillow.com/is/image/i0/i0/i158/ISz37jvwid0rz7.jpg>)

2. Bungalow⁵

⁴ For more information, please see: <http://architecture.about.com/od/periodsstyles/ig/House-Styles/Folk-Victorian.htm>

⁵ For more information, please see: <http://architecture.about.com/od/periodsstyles/ig/House-Styles/Bungalow-Styles.htm>

The Bungalow is an architectural style that was popularized by Charles Sumner Greene and Henry Mather Greene in the early 20th Century. Bungalows tend to reflect a host of different architectural styles, but are typically characterized by their horizontal shape, low pitched roofs, and one- or one and a half-story floor plan. Bungalows are typically constructed of wood; however, stone bungalows are not uncommon. Many of the bungalows within the Study Area are constructed of a mix of wood and masonry materials, and contain one and a half stories as well as porches.



Figure 8: Bungalows within the Study Area are typically one story in height and contain additional floor area above the first floor, as well as porches. (Source: T&M Associates)



Figure 9: Another style of Bungalow in the historic Houston Heights Neighborhood, Houston, Texas (Source: http://www.houstonheights.org/newsletter/2006-11/November_2006_NL%20Accessories/media/home_1829_Cortlandt.gif)

3. Dutch Colonial⁶

⁶ For more information, please see: <http://www.antiquehome.org/Architectural-Style/dutch-colonial.htm>

The Dutch Colonial style of homes is a subtype of the revival of Colonial style architecture that became popular during the first half of the 20th Century. The most characteristic feature of Dutch Colonial homes is the gambrel roof—a low, sweeping, roofline that allows for extra living space on a second story. Dutch Colonials also typically feature symmetrical facades, clapboard or shingle siding, and porches underneath overhang eaves. Dutch Colonials are typically constructed of masonry or wood, and often contain dormer windows similar to those present in bungalows. There are several Dutch Colonial homes located within the Study Area.



Figure 10: A Dutch Colonial home located within the Study Area
(Source: T&M Associates)



Figure 11: Gambrel roofs are the trademark characteristic of Dutch Colonial homes.
Source:(<http://media.imaxws.net/181/10brookledgeext.jpg>)

B. Building Elements for Residential Buildings

Newly constructed residential dwellings shall follow the subsequent prescribed guidelines:

1. Roofs

The roofs of residential buildings shall be symmetrical hips or gables with an overhang of at least twelve (12) inches and a pitch of between 4:12 and 12:12. Overhangs shall not exceed thirty (30) inches beyond the face of any façade.

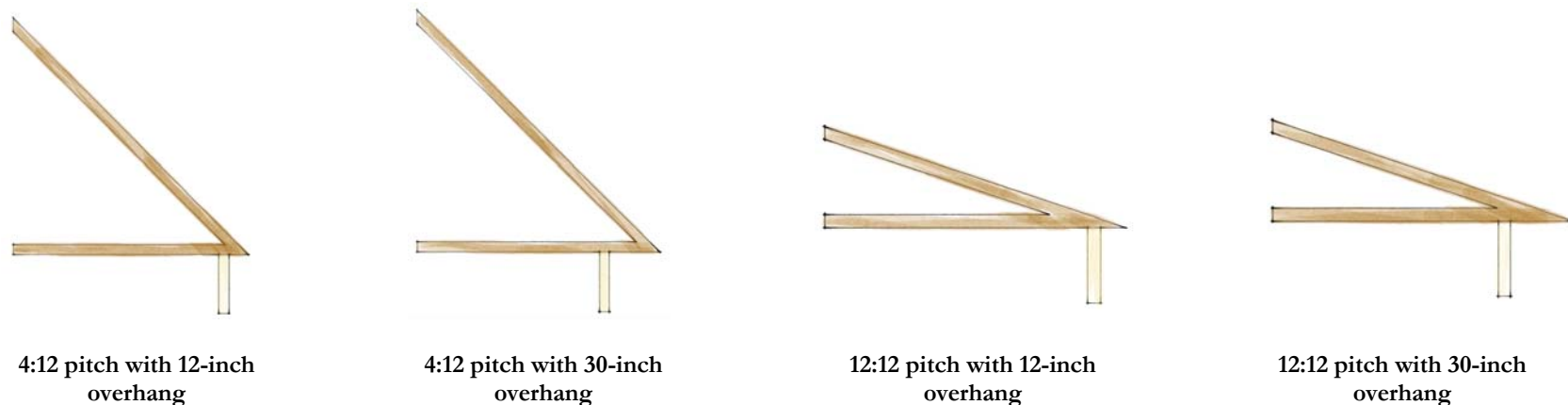


Figure 12: Cross sections of minimum and maximum permitted roof pitches and overhangs.
(Source: T&M Associates)

In addition, roof forms for new construction and restoration shall follow the following guidelines:

- Historic roof lines and shapes and forms such as decorative chimneys, weather vanes, cupolas, towers, and ornamental roof details which are consistent with the architectural styles and objectives of the Transit Village Study Area should be replicated or retained;
- The design of all new gable roofs should observe proportions and roof pitch consistent with period prototypes;
- Roof dormers, eyelids or other similar roof elements consistent with architectural ornamentation and style are encouraged;

- Standing steam metal, formed copper, slate, wood shake or dimensional asphalt shingle shall be utilized on all pitched roof surfaces. Other synthetic roofing materials such as cement, polymer or vinyl composition which simulate wood or slate shingles are also acceptable;
- The placement of TV satellite and other mechanical HVAC devices on roofs shall be placed in a manner invisible to public view;
- The removal or alteration of historic or original roof overhangs, dormers, gables, soffits, eyelids and towers should be avoided wherever possible. Additionally, new constructions and renovations should reconstitute, reconstruct, or incorporate such new elements as features.

2. Walls

Walls may be constructed of two different materials provided that the heavier material is placed below. When two materials are used on a front façade and terminate on the outside corner of the façade, both materials must continue for a minimum distance of three (3) feet around the corner of the side façade. At the joint between a wall and an overhanging pitched roof, the use of moldings is appropriate to indicate the transition. Uninterrupted, long and undefined walls are not desirable and shall be avoided wherever possible. Articulations, setbacks, vertical and horizontal separations and delineation by material change greatly enhance general interest and visual appeal, and are therefore encouraged.

3. Windows and Doors

Windows and doors shall be consistent with building scale and function. All windows and doors shall comply with the Township Construction Code. In addition, all window and door frames shall be either stained or painted in final appearance. Unfinished brushed aluminum or unpainted steel window and door frames are not permitted.

Any window or through-wall mounted air conditioning units are strongly discouraged in all new construction. Additionally, existing structures utilizing window air-conditioning units should position units away from street and public view to the extent feasible. Where such placement is necessary, units shall be adequately screened and/or positioned in rear yard or roof areas.

4. Chimneys

Chimneys shall be located so as to have a minimal distance between the peak roof point or ridge line of the structure and the chimney cap. Exterior chimneys shall be finished in brick or stone.

5. Porches

Porches are required to be incorporated into the design and construction of new residential dwellings. Porches enhance the character of residential neighborhoods by providing a transition between public and private space, providing additional outdoor living area for residents, and encouraging informal social interaction between neighbors. In addition, porches are an integral characteristic of the existing residential neighborhoods within the Study Area. Front yard decks are not permitted within the Study Area. In addition, pressure treated lumber shall not be used to construct porches within the Study Area.

6. Materials

Choice of material shall be based upon sustainability and the green-rating of the material. When choosing building materials, the following should be considered:

- Whether the material is locally produced or indigenous to the area;
- Whether the material is manufactured from a renewable source;
- Whether the material is oil-based;

Vinyl siding in short segments that may result in checkerboard patterns and laps shall not be permitted within the Study Area. Rather, the exterior of residential dwellings shall be finished with seamless horizontal vinyl clap board siding or vertical trim boards or another acceptable material.

6. Fencing and Landscaping

Front yard fences shall be made of wood picket, split rail, or decorative metal such as wrought iron. Solid or chain link fencing within front yard areas is not permitted. Front yard fences may not exceed four (4) feet in height and shall have a minimum 50 percent opening. Side and rear yard fences may match front yard fences, or may be chain link fencing, board-on-board fencing, or another similar material. Side and rear yard fencing shall not exceed six (6) feet in height.

Fencing shall be finished in earth-tone colors. Acceptable colors include dark green, black, gray, brown, or white. The painting or staining of existing fencing is acceptable and encouraged. Unfinished metal fencing, multi-colored fencing, fencing utilizing patterned colors, and the use of bright, non-earth tone colors are not permitted.

Retaining or decorative walls may be of brick or stone and must match the main building.

7. Private Frontages

In addition to porches, which are required to be constructed with new residential dwellings, the following private frontage types are permitted for new residential structures:

- Common Yards
- Fences (must conform to the above requirements)
- Terraces or Dooryards
- Forecourts
- Stoops

C. General Requirements for Non-Residential and Mixed-Use Buildings

Newly constructed non-residential and mixed-use buildings within the Study Area shall be designed in a manner that is consistent with the Study Area's industrial past. Newly constructed non-residential and mixed-use buildings shall emulate the architectural elements of a turn-of-the-century factory building. Alternatively, planned non-residential and mixed-use buildings shall be designed to emulate the architecture of a suburban downtown area.

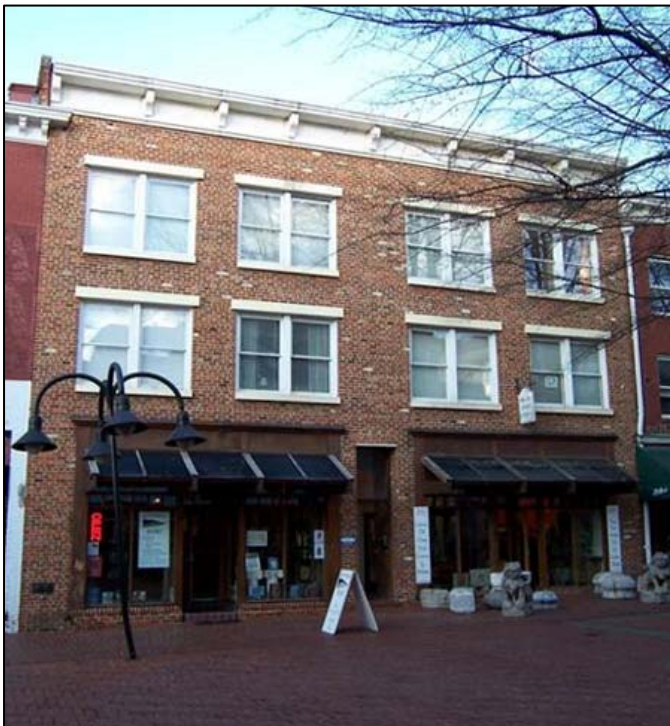


Figure 13: Non-residential and mixed-use buildings shall be designed to incorporate turn-of-the-century architectural elements
(Source: <http://www.cbre.com/NR/rdonlyres/EC6D7A19-0647-4C3A-80DC-99E6C337533E/554018/111115WMainSt.jpg>)



Figure 44: Non-residential and mixed-use buildings may also be designed to emulate suburban downtown architecture (Source: <http://www.fototime.com/photos/st/479BB44CE33D4DF5A75CD8DC8DC4CDA3/Downtown%20Metuchen%20NJ%20Saturday%20AM9.jpg>)

D. Building Elements for Non-Residential and Mixed-Use Buildings

Newly constructed non-residential and mixed-use buildings shall follow the subsequent prescribed guidelines:

1. Materials

Non-residential and mixed-use buildings within the Study Area shall be constructed of brick, cast concrete, or some other masonry material that is similar in appearance and durability. All ancillary structures shall be constructed of materials similar in appearance to the primary structure. Commercial and mixed-use buildings are encouraged to incorporate some element of ornamentation on building facades that front public streets. However, ornamentation is not a required element of design.

2. Roofs

Main roofs on commercial and mixed-use buildings shall be constructed as symmetrical hips or gables, wherever possible. Roofs shall be designed to incorporate an overhang of at least twelve (12) inches and a pitch of between 4:12 and 12:12 (See Figure 12). Overhangs shall not exceed thirty (30) inches beyond the face of any façade.

Flat roofs are permissible on non-residential and mixed-use buildings. Flat roofs must be raked at the minimum slope necessary to shed water and meet other standard construction requirements.

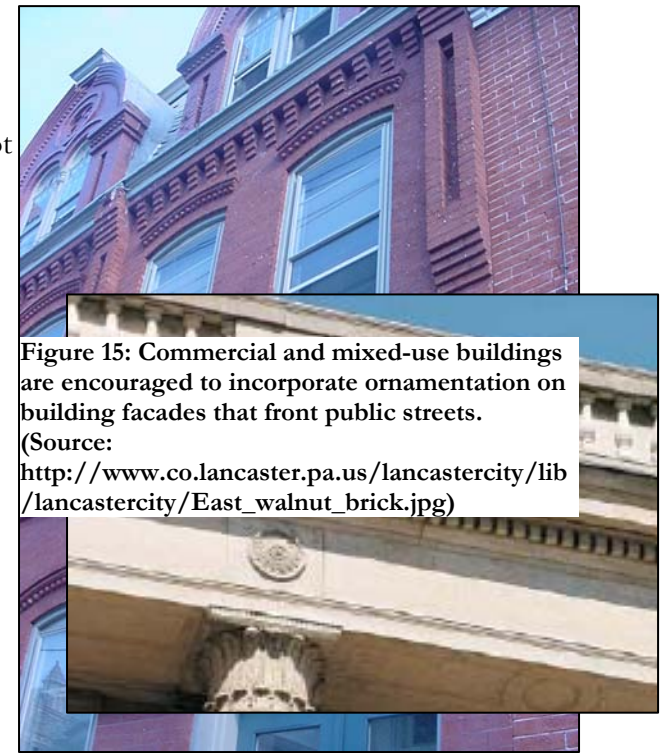


Figure 15: Commercial and mixed-use buildings are encouraged to incorporate ornamentation on building facades that front public streets.
(Source: http://www.co.lancaster.pa.us/lancastercity/lib/lancastercity/East_walnut_brick.jpg)

Buildings with flat roofs must include a parapet surround on the primary (front) façade, as well as on both side facades. The top of the parapets shall be no lower than eighteen (18) inches higher than the adjacent roof surface.

3. Windows and Doors

Window types shall include bay, dormer, and double hung windows. Single-pane and tilt windows are permitted provided that the glass panes have mullions within. The minimum vertical proportion of a window shall be 1.5 times its width. Bands of windows (a horizontal series of three windows or more, separated only by mullions that form a horizontal band across the façade of a building) are permitted.

Glass types for windows shall be clear. Window treatments shall include caps, lintels, and sills.

Permitted door types include entry doors and French doors for public entrances into non-residential (e.g. commercial) uses. For entrances to private residences that may be located within mixed-use buildings, only entry doors shall be permitted. French doors and patio doors may be permitted to provide access to any patios that may be located at the rear of any non-residential or mixed-use building.

4. Chimneys

Figure 16: Parapets are required architectural elements on non-residential and mixed-use buildings that incorporate flat roofs (Source: Town of Redington Shores, Florida Urban Design Guidelines)

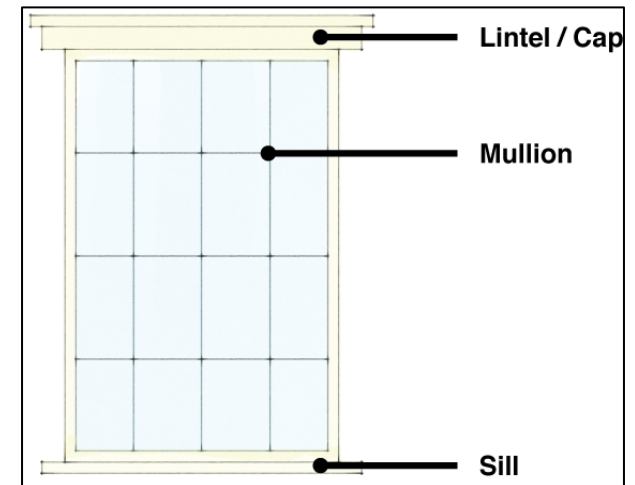


Figure 17: Window treatments shall include lintels/caps, mullions, and sills. A window's height shall be at minimum 1.5 times its width (Source: T&M Associates)

Exterior chimneys shall be finished in brick or stone. Chimney hoods and caps shall be finished with the same material as the chimney shaft. Chimney shafts shall be integrated into the design of the building.

5. Porches and Awnings

All structures built within Form Area C are required to include physical extensions that extend into the intermediate space between the required minimum setback and the public right-of-way. These physical extensions are intended to provide a transition between public and private space, to provide pedestrians with protection from the sun and rain, and to enhance the experience of pedestrians who walk along Memorial Drive. These physical extensions may be in the form of porches or awnings.

At a minimum, awnings shall be constructed of a rigid metal frame with a stretched canvas cover. However, awnings may be designed to incorporate ornate architectural details that are consistent with the overall character of the subject building, as well as the surrounding buildings.

Porches shall be constructed of elements that are consistent with the overall architectural character of the subject building. Porches shall include vertical supports as well as horizontal extensions from the building façade in order to create an effective outdoor space that includes a roof, a floor, and three open sides. Porches may be constructed to allow additional usable space above the ground floor (e.g. balconies). In these cases, a traditional porch roof may be replaced with a

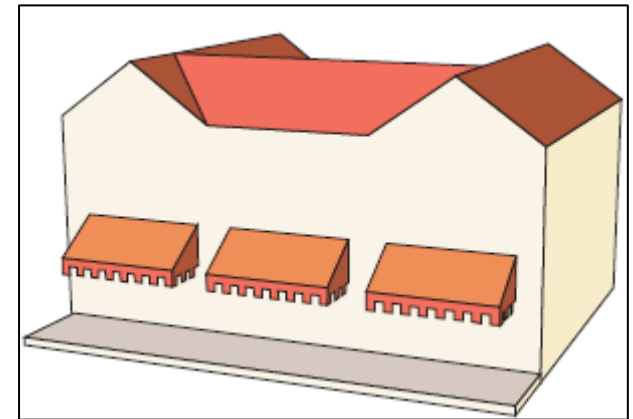
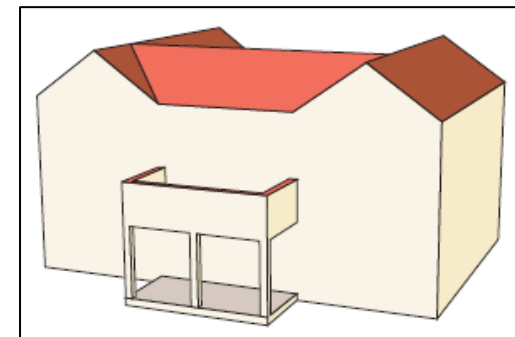


Figure 18: At minimum, awnings shall be provided on the primary facade of all buildings within Form Area C. (Source: Town of Redington Shores, Florida Urban Design Guidelines)



usable floor to provide accessible outdoor space for residents or tenants of the second story of the building. This technique may also be replicated on the third and fourth stories of a building within Form Area C.

All buildings that front Memorial Drive are required to provide either a porch or awnings that shall extend from the primary façade of the building. At a minimum, porches and awnings are required to be no less than one-third (1/3) of the length of the front façade of the building to which they are attached, and shall provide protection for all the entryways of the building. It is strongly encouraged, though not required, that additional protection be provided for all building openings, including windows.

Figure 19: An example of a building with a porch attached to its front facade. Porches may be used to create additional usable outdoor space for floors above the ground floor. (Source: Town of Redington Shores, Florida Urban Design Guidelines)

Awnings shall extend no less than six (6) feet from the front façade of a building, and porches shall extend no less than seven (7) feet from the front façade of a building.

6. Private Frontages

In addition to porches and awnings, which are required to be constructed with new non-residential or mixed-use buildings that front Memorial Drive, the following private frontage types are permitted for new structures:

- Terraces or Dooryards (at the rear of the building only)
- Stoops

3. Public Frontages and Thoroughfare Assemblies

The following section provides design guidelines for the public realm of the Study Area, specifically streets and sidewalks. The intent of this section is to provide guidelines and regulations that, when implemented, will allow for the creation of an attractive and safe public realm that minimizes conflicts between pedestrians, bicyclists, and motorists.

Public frontage refers to the area between the front lot line of a parcel or group of parcels, and the beginning of street pavement. Public frontage includes sidewalks, planting strips, and curbs. Thoroughfare assembly refers to the configuration and design of a dedicated right-of-way, including roadways, and the elements that make up public frontage.

In order to establish guidelines for thoroughfares within the Study Area, they have been divided into three distinct groups: Memorial Drive; 5th, 6th, 7th, 8th and 9th Avenues; and Atkins Avenue. The design guidelines for each of these groups are described below.

A. Memorial Drive

Memorial Drive is the main thoroughfare within the Study Area. Much of the Area's future non-residential and mixed-use development will be concentrated along the Memorial Drive Corridor. Therefore, it can be expected that the Memorial Drive Corridor will be a high traffic area where users will move by several different modes of transportation. The design guidelines for the Memorial Drive Corridor are as follows:

1. Assembly

The assembly width for Memorial Drive shall be no less than fourteen (14) but no more than twenty (20) feet in width.

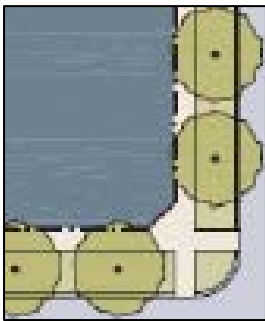


Figure 20: Assembly space includes all public right-of-way space between front lot lines and street pavement
(Source: Duany Plater-Zyberk & Co.)

2. Curb Type and Radius

Raised curbs shall be required along Memorial Drive. Curb Radii along Memorial Drive shall be no less than five (5) feet, but no more than twenty (20) feet in length.

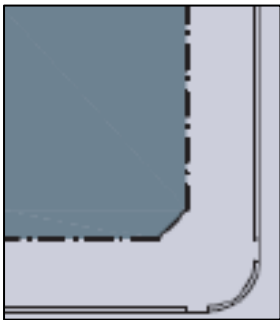


Figure 21: The length of curb radii determines the speed at which a motorist is able to safely make a turn. The shorter the curb radius, the more a motorist must slow down in order to safely make a turn
(Source: Duany Plater-Zyberk & Co.)

3. Walkway Type and Width

Sidewalks shall be provided along Memorial Drive. Sidewalks shall be no less than eight (8) feet but no more than twelve (12) feet in width.

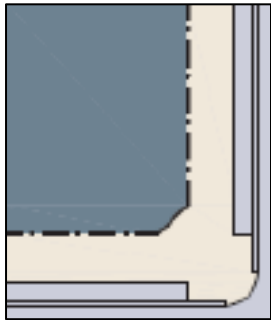


Figure 22: Wide sidewalks shall be provided along Memorial Drive to encourage pedestrian circulation through the corridor. (Source: Duany Plater-Zyberk & Co.)

4. Planter Guidelines

Planting strips shall be provided along Memorial Drive to beautify the corridor and buffer pedestrians from vehicular and bicycle traffic. Planting strips shall be no less than five (5) feet and no more than twelve (12) feet in width and shall be continuous, breaking only for driveways and ADA-accessible curb ramps provided for pedestrian crossing.

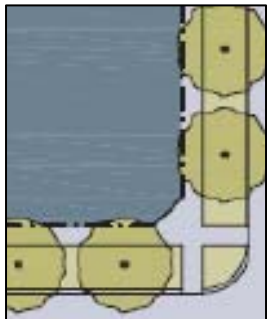


Figure 23: Planters shall be provided along the Memorial Drive Corridor to beautify the corridor and ensure pedestrian safety. (Source: Duany Plater-Zyberk & Co.)

Planting strips shall incorporate shade trees to provide pedestrians with protection from the sun, and to enhance the aesthetics of the

Memorial Drive Corridor. Shade trees shall be planted on center within the planting strip with twenty-five (25) feet between plantings.

Shade trees planted along the Memorial Drive Corridor shall be all the same species. Suggested species include:



Autumn Blaze Red Maple
(Source: <http://www.fast-growing-trees.com/ShadeTrees.htm>)



October Glory Maple
(Source: <http://www.fast-growing-trees.com/ShadeTrees.htm>)



Crimson King
(Source: <http://www.fast-growing-trees.com/ShadeTrees.htm>)

5. Lighting

The Memorial Drive Corridor shall be a well-lit area to ensure pedestrian and vehicular safety, as well as pedestrian security during nighttime hours. In addition, implementing the use of decorative street lights has the potential to enhance the aesthetics of Memorial Drive.

Light fixtures along Memorial Drive shall be pipe, post, column, or double column fixtures and shall not exceed fourteen (14) feet in height. Fixtures shall be placed on the sidewalk in close proximity to the planting strip. Fixtures shall be placed evenly and occur at

intervals of fifty (50) feet on the west side of Memorial Drive, and at 100-foot intervals along the east side of Memorial Drive. Special considerations may be given to the presence of outdoor seating areas for restaurants, the presence of traffic signals, and/or utility poles when placing light fixtures.



Pipe



Post



Column



Double
Column

Figure 24: Light fixture types permitted along Memorial Drive
(Source: T&M Associates)

6. Street Furniture

Street furniture shall be provided along the Memorial Drive Corridor. Street furniture shall be coordinated in color and style, and shall include benches, trash receptacles, and newspaper vending machines at a minimum. Street furniture shall be placed opportunistically between lighting fixtures, or on small concrete pads that may be located on the planting strip. Benches shall be placed in close proximity to street trees wherever possible. Trash receptacles shall be placed on or near street corners. Newspaper vending machines may be placed opportunistically.



Figure 25: Street furniture shall be coordinated in style and placed opportunistically on sidewalks or on concrete pads within the planting strip, as seen here (Source: Town of Redington Shores, Florida Urban Design Guidelines)

B. 5th, 6th, 7th, 8th, and 9th Avenues

5th, 6th, 7th, 8th, and 9th Avenues are the primary residential streets within the Study Area, and therefore, it can be expected that a substantial amount of the Area's future residential development will occur along these roadways. The design guidelines that follow are intended to preserve and enhance the existing character of the Area's residential streets.

Two improvement projects have been scheduled to improve 5th and 6th Avenues, and the Township has applied for a grant to fund future improvements to 7th Avenue. The Township, in conjunction with the Borough of Neptune City, will be making curb, sidewalk, and road surface improvements to the 5th Avenue Corridor from Route 35 to Memorial Drive, which includes the segment of 5th Avenue that is contained within the Study Area. In addition, Neptune Township plans to use Community Development Block Grant (CDBG) and additional Township funding to construct street improvements to 6th Avenue that includes the installation of new paver or concrete walkways, ADA accessibility improvements, as well as landscaping and lighting improvements. Finally, the Township has applied for 2010 New Jersey Department of Transportation (NJDOT) Safe Routes to Transit funding to construct curb, sidewalk, accessibility, and lighting improvements along 7th Avenue.

1. Assembly

The assembly widths for 5th, 6th, 7th, 8th, and 9th Avenues shall be no less than ten (10) but no more than sixteen (16) feet in width (See Figure 20).

2. Curb Type and Radius

Raised curbs shall be required along 5th, 6th, 7th, 8th, and 9th Avenues. Curb Radii along these thoroughfares shall be no less than five (5) feet, but no more than twenty (20) feet in length (See Figure 21).

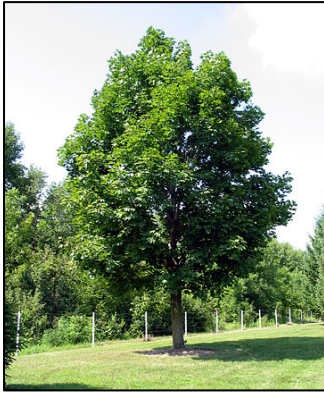
3. Walkway Type and Width

Sidewalks shall be provided on both sides of 5th, 6th, 7th, 8th, and 9th Avenues. Sidewalks shall be no less than four (4) feet but no more than eight (8) feet in width (See Figure 22).

4. Planter Guidelines

Planting strips shall be provided along both sides of 5th, 6th, 7th, 8th, and 9th Avenues, and will function to beautify the streets and buffer pedestrians from vehicular traffic. Planting strips shall be no less than five (5) feet and no more than twelve (12) feet in width, and shall be continuous, breaking only for driveways and ADA-accessible curb ramps provided for pedestrian crossing (See Figure 23).

Planting strips shall incorporate shade trees to provide pedestrians with protection from the sun, and to enhance the aesthetics of the residential streets. Shade trees shall be planted on center within the planting strip at intervals of thirty (30) feet. Shade trees planted along 5th, 6th, 7th, 8th, and 9th Avenues shall be alternating species. Suggested species include:



Norway Maple
(Source:
[http://www.pgandd.com/
Norway_maple.jpg](http://www.pgandd.com/Norway_maple.jpg))



Red Maple
(Source:
<http://www.charactertrees.com/sitebuilder/images/RedMaple-450x600.jpg>)



Red Oak
(Source:
<http://www.capeshoregardens.com/native/Red%20Oak001.jpg>)



Green Ash (Source:
<http://extension.missouri.edu/explore/images/g06800greenash01.jpg>)

5. Lighting

5th, 6th, 7th, 8th, and 9th Avenues shall be well lit streets to ensure pedestrian and vehicular safety, as well as pedestrian security during nighttime hours. In addition, implementing the use of decorative lighting fixtures will enhance the aesthetics of these residential streetscapes.

Light fixtures along 5th, 6th, 7th, 8th and 9th Avenues shall be pipe, post, or column fixtures and shall not exceed fourteen (14) feet in height. Fixtures shall be placed on concrete pads within the planting strip, and shall be placed evenly at intervals of one hundred (100) feet on alternating sides of the roadway. Special considerations may be given to the presence of traffic signals, and/or utility poles when placing light fixtures.



Pipe



Post



Column

Figure 26: Light fixture types permitted along Atkins Avenue
(Source: T&M Associates)

C. Atkins Avenue

Atkins Avenue forms the western boundary of the Study Area and functions primarily as a residential street, though a small number of non-residential land uses are located along Atkins Avenue. Although Atkins Avenue is a residential street, the street sees a substantial amount of through traffic. The design guidelines that follow are intended to preserve and enhance the existing character of Atkins Avenue.

1. Assembly

The assembly widths for Atkins Avenue shall be no less than ten (10) and no more than sixteen (16) feet in width (See Figure 20).

2. Curb Type and Radius

Raised curbs shall be required along Atkins Avenue. Curb Radii along Atkins Avenue shall be no less than five (5) feet and no more than twenty (20) feet in length (See Figure 21).

3. Walkway Type and Width

Sidewalks shall be provided on both sides Atkins Avenue. Sidewalks shall be no less than six (6) feet and no more than ten (10) feet in width (See Figure 22).

4. Planter Guidelines

Planting strips shall be provided along both sides of Atkins Avenue, and will function to beautify the street and buffer pedestrians from vehicular traffic. Planting strips shall be no less than five (5) feet and no more than twelve (12) feet in width and shall be continuous, breaking only for driveways and ADA-accessible curb ramps provided for pedestrian crossing (See Figure 23).

Planting strips shall incorporate shade trees to provide pedestrians with protection from the sun, and to enhance the aesthetics of the Avenue. Shade trees shall be planted on center with thirty (30) feet between plantings. Shade trees planted along Atkins Avenue shall be alternating species. Suggested species include:



Norway Maple
(Source:
[http://www.pgandd.com/
Norway_maple.jpg](http://www.pgandd.com/Norway_maple.jpg))



Red Maple
(Source:
<http://www.charactertrees.com/sitebuilder/images/RedMaple-450x600.jpg>)



Red Oak
(Source:
<http://www.capeshoregardens.com/native/Red%20Oak001.jpg>)

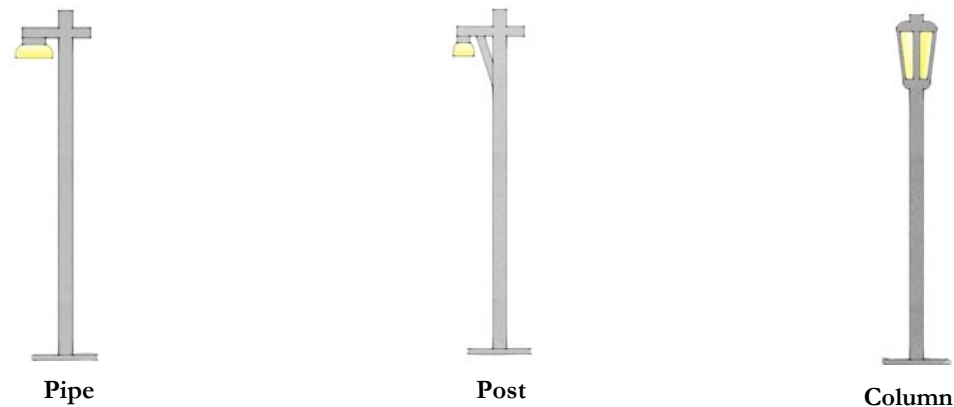


Green Ash (Source:
<http://extension.missouri.edu/explore/images/g06800greenash01.jpg>)

5. Lighting

Atkins Avenue shall be a well-lit street to ensure pedestrian and vehicular safety during nighttime hours. In addition, implementing the use of decorative lighting fixtures will enhance the aesthetics of the Atkins Avenue streetscape.

Light fixtures along Atkins Avenue shall be pipe, post, or column fixtures and shall not exceed fourteen (14) feet in height. Fixtures shall be placed on concrete pads within the planting strip and shall be placed at intervals of seventy-five (75) feet on both sides of the roadway. Special considerations may be given to the presence of traffic signals, and/or utility poles when placing light fixtures.



**Figure 27: Light fixture types permitted along Atkins Avenue
(Source: T&M Associates)**

6. Street Furniture

Street furniture is not required along the Atkins Avenue Corridor. However, the provision of benches and trash receptacles are encouraged. Street furniture shall be coordinated in color and style, and shall be placed opportunistically on small concrete pads that may be located on the planting strip. Benches shall be placed in close proximity to street trees wherever possible and trash receptacles shall be placed on or near street corners.

D. Traffic Calming

According to the Institute of Transportation Engineers (ITE), “traffic calming is the combination of mainly physical measures to reduce the negative effects of motor vehicle use, alter driver behavior and improve conditions for non-motorized street users.”⁷ Implementing traffic calming measures is an essential action in creating safe streets for pedestrians, and limiting potentially dangerous conflicts between vehicles, bicyclists, and pedestrians. The subsections that follow discuss traffic calming mechanisms that may be implemented within the Study Area to achieve these goals.

1. Bump outs

Bump outs, also known as neckdowns, involve the extension of the curb line to narrow the street width and shorten the length of the crosswalk, as noted in Figure 28. Bump outs reduce the curb-to-curb roadway width, making the intersection more pedestrian friendly by shortening the crossing distance, and improving the visibility of the crosswalk and the intersection via raised peninsulas. Bump outs can also tighten curb radii, thereby reducing the speed of turning vehicles.

Generally, the curb is extended eight (8) feet from the existing curb line, thereby creating a protected area for parked vehicles. This measure also provides greater

⁷ I.M. Lockwood, “ITE Traffic Calming Definition,” *ITE Journal*, Vol. 67, July 1997, page 22-24

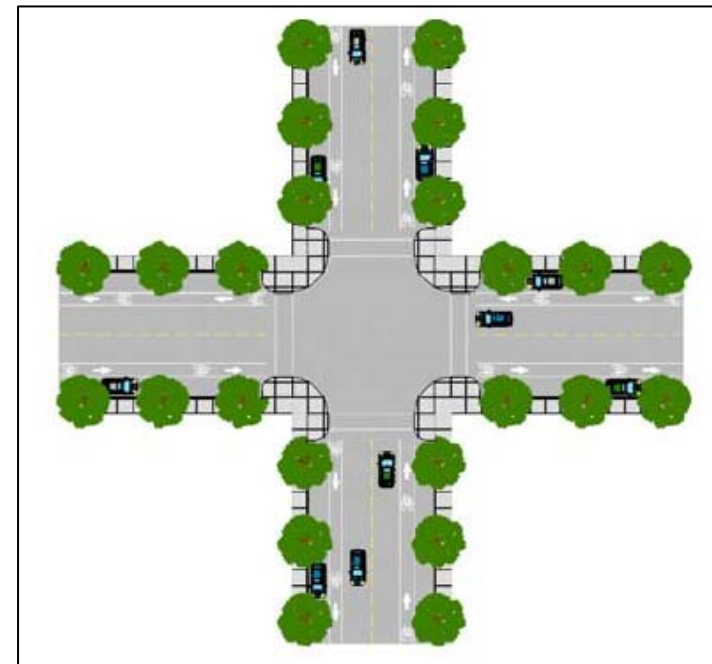


Figure 28: Bump outs shorten curb to curb pavement width, thereby allowing pedestrians to cross a roadway faster than if conventional curb cuts are provided. (Source: T&M Associates)

visibility at intersections, since vehicle travel away from the curb line and closer to the centerline of the traveled way. Bump outs reduce the length of the crosswalk for pedestrians, while creating a narrowing effect to the motorist traveling through the bump out. Bump outs may also be used at mid-block locations to provide areas for benches and plantings. Vertical elements such as flagpoles, trash cans, and street trees can also be used to draw attention to the constriction of the road and provide a visual cue to the motorist.

Bump outs may be most effective if they are provided along the Memorial Drive Corridor. Citizen feedback suggests that cars traveling along the Memorial Drive Corridor do so at excessive speeds, making it difficult for pedestrians to safely cross the roadway. Bump outs should also be considered along the Atkins Avenue Corridor, where a wide cartway width and a straight roadway allow motorists to travel at excessive speeds.

2. Textured Pavement and Crosswalks

Concrete pavers, stamped concrete, and/or decorative pavement can be placed at intersections to warn motorists that they are approaching a pedestrian crossing (see Figure 29). This traffic calming measure by itself has a minimal effect on reducing traffic speeds or volume. However, it is particularly effective when used in combination with other traffic calming measures.

Textured pavement and crosswalks may present traction problems for bicyclists and wheelchairs if a heavily-textured surface is utilized. Care should be taken to design an appropriate pavement texture that meets ADA requirements. Consideration

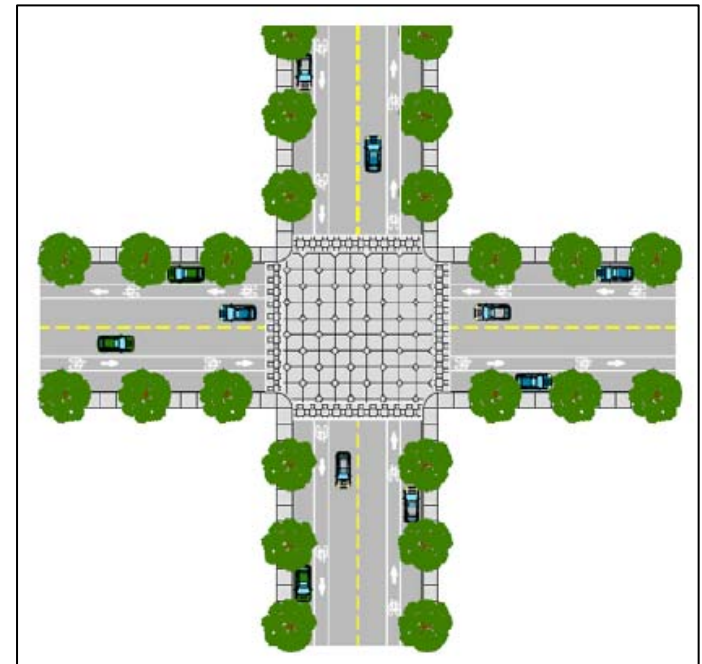


Figure 29: Textured pavement and crosswalks warn motorists of an upcoming pedestrian crossing, and enhance the aesthetics of roadways on which they are implemented. (Source: T&M Associates)

should also be given to bicyclists when developing a textured pavement treatment. Another issue with implementing textured pavement at crosswalks is associated with roadway maintenance, particularly as it relates to utility work and road opening permits. When a road opening is required in areas having textured or decorative pavement, the roadway will only be patched with hot mix asphalt. Therefore, this traffic calming strategy should be implemented concurrently with a road opening permit ordinance that requires a contractor to restore the road surface using a matching decorative pavement.

Textured pavement and crosswalks should be implemented in intersections along the Memorial Drive Corridor. Implementing these traffic calming measures would reduce the overall speed of automobiles traveling along the roadway, and would also provide an added element of beautification to the Study Area.

3. Raised Medians and Pedestrian Refuges

A raised median includes the installation of a curbed island within the center of a street. This island narrows the travel lanes at that location and provides pedestrians with a refuge area at the center of the street. This reduces crossing distance for pedestrians by allowing them to cross half of the street at one time, as shown in Figure 30. They can be provided at approaches to intersections or along an entire block.

Median islands should be at least 6 feet wide and should be at least 20 feet in length. Access across the raised median should be provided in the design to

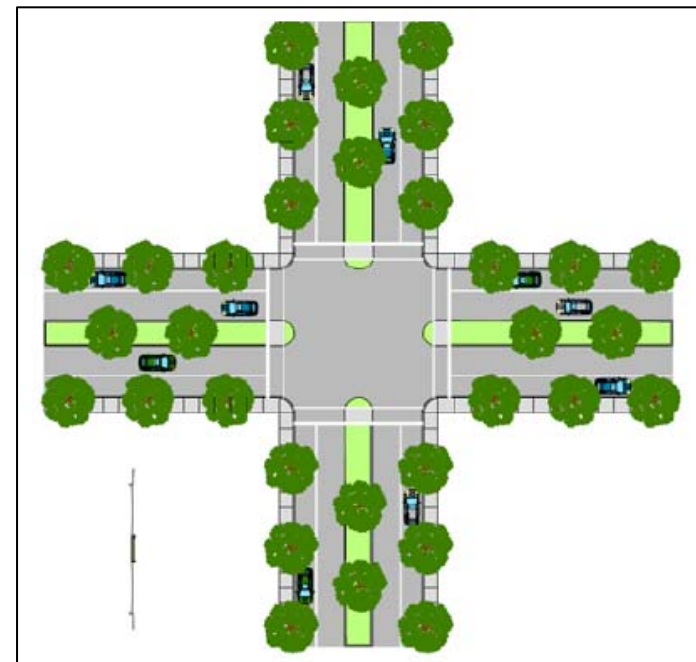
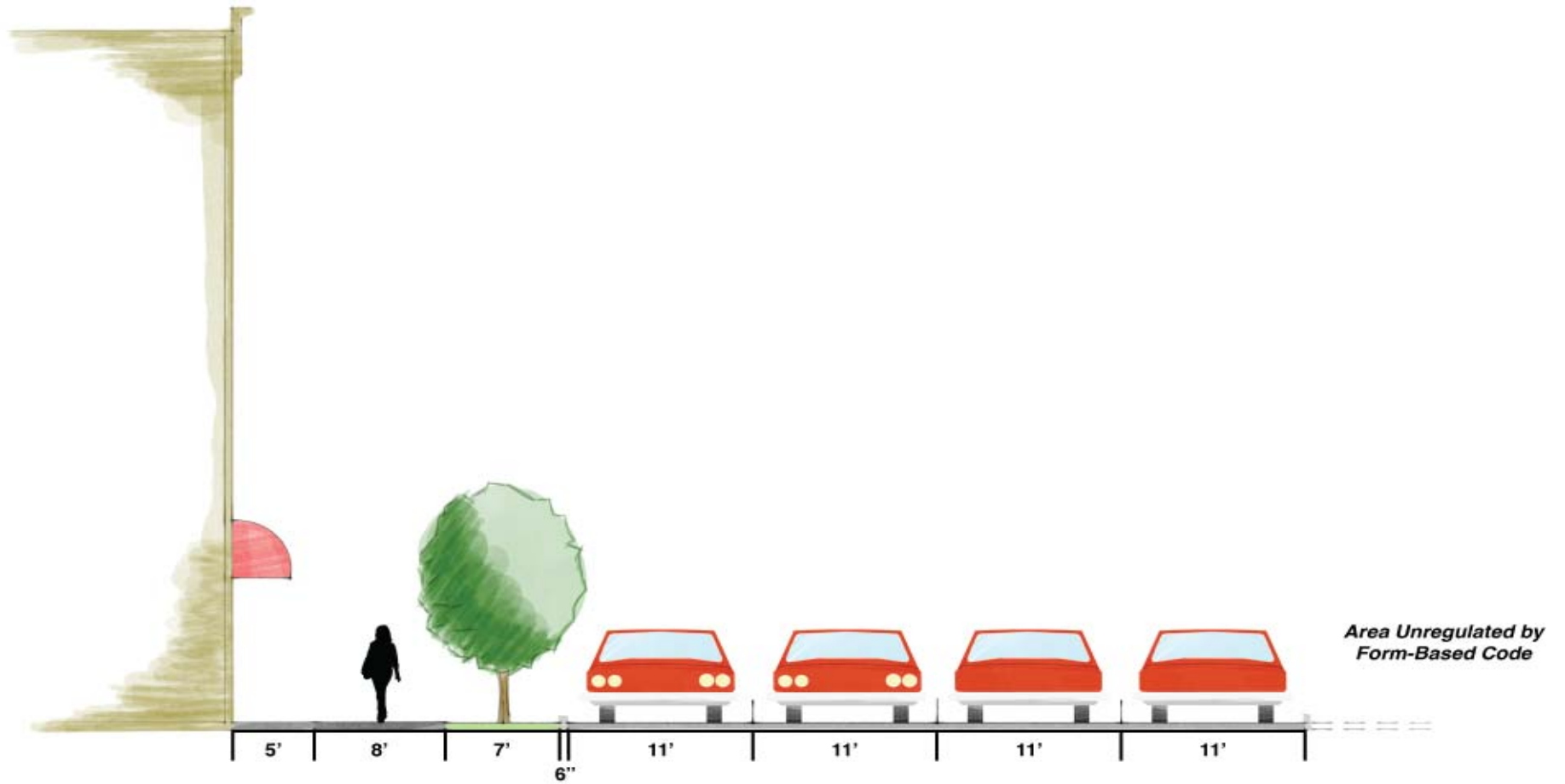


Figure 30: Raised medians and pedestrian islands narrow travel lanes and provide pedestrians with a refuge to cross busy streets. (Source: T&M Associates)

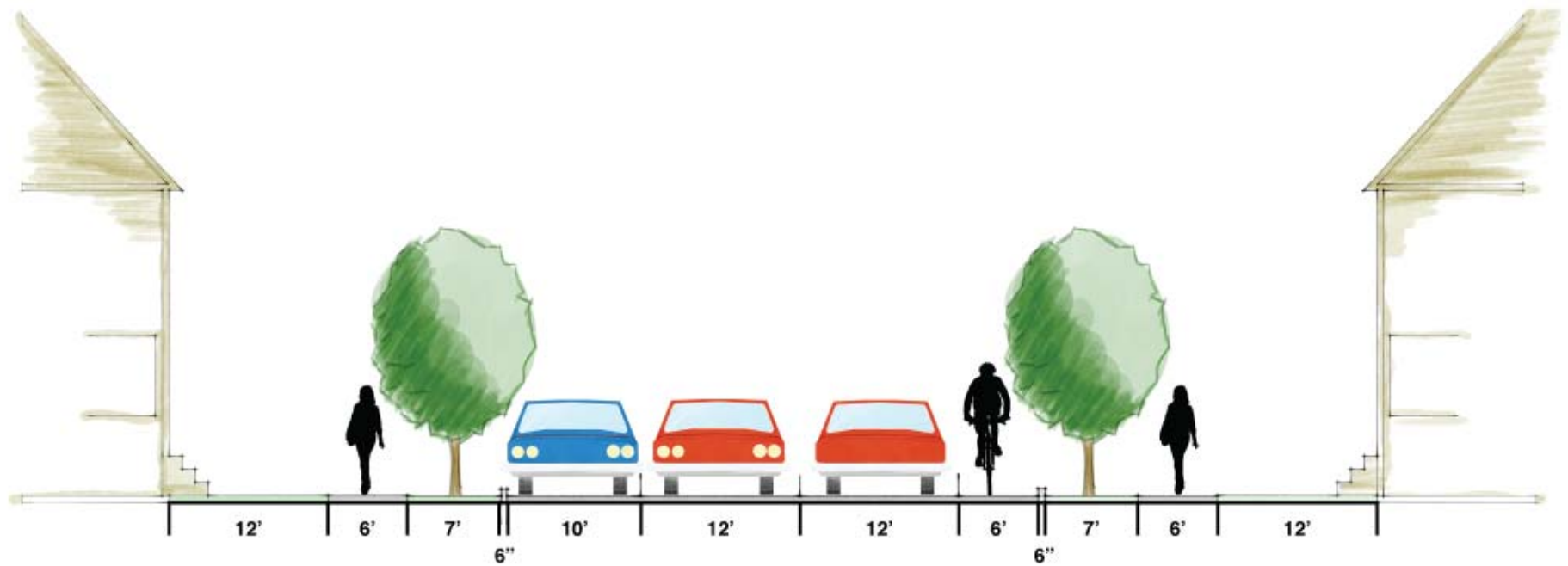
accommodate pedestrians and bicyclists. Raised medians will restrict access to driveways to/from one direction of travel. Raised medians and pedestrian refuges would be most appropriate along the Memorial Drive Corridor. Implementing these traffic calming measures would reduce the overall speed of automobiles traveling along the roadway, and would also provide an added element of beautification to the Study Area. Any raised medians should be appropriately landscaped, but should not interfere with the intended function of the traffic calming measure.

E. Thoroughfare Assemblies

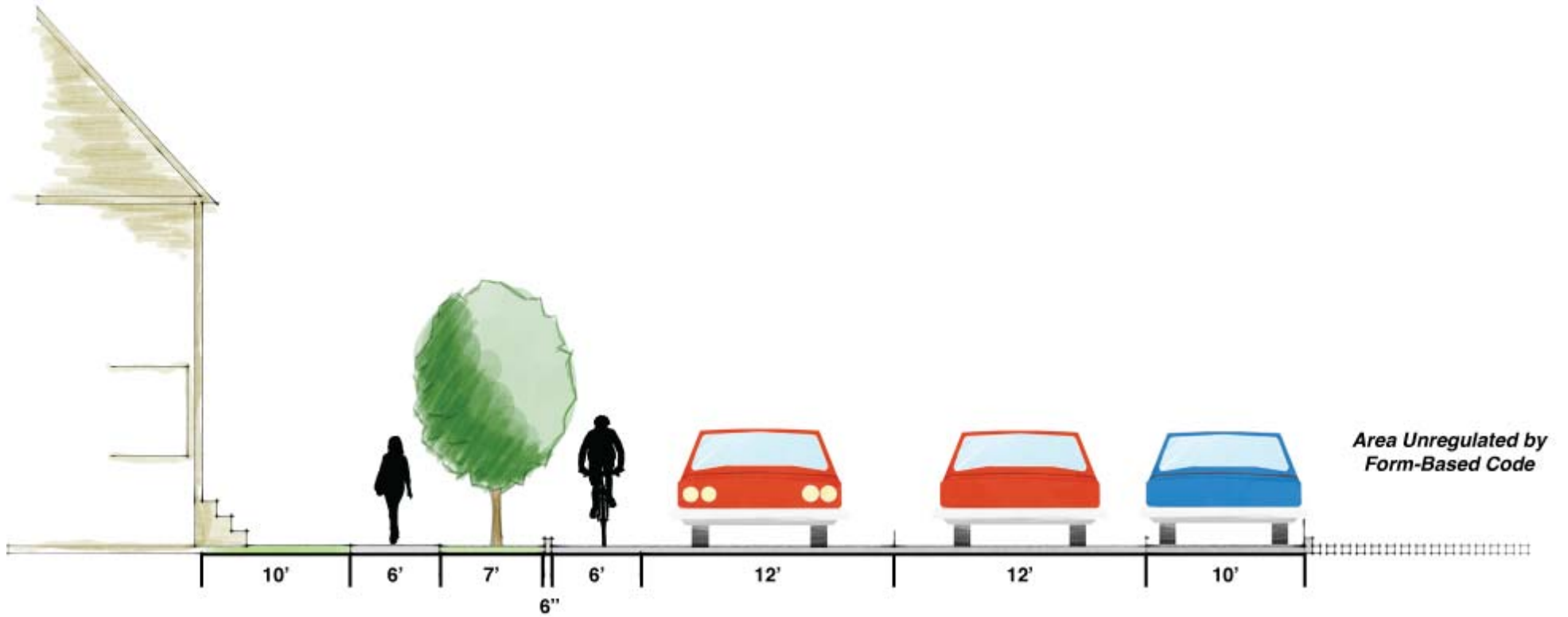
1. Memorial Drive



2. 5th, 6th, 7th, 8th, and 9th Avenues



3. Atkins Avenue



F. Parking & Signage

The following section is intended to regulate parking and signage within the Study Area. The section takes into account the existing and intended character of the Study Area, as well as community input solicited at the community visioning workshops.

1. Parking Placement and required dimensions

Parking areas shall be located at the rear of buildings wherever possible in order to preserve an attractive, walkable public realm. If locating parking areas at the rear of buildings is not possible, parking areas may be provided beside structures, provided that the parking area does not occupy a distance of more than one-third (1/3) of the entire lot or block frontage, whichever is smaller.

No parking areas shall front on Memorial Drive.

All off-street parking provided for multifamily residential uses shall be located at the rear of the structure. Parking areas provided for single-family structures may be provided by a driveway located on the subject lot.

Required dimensions for parking areas, associated driveways, and the number of required parking stalls located within the Study Area shall be governed by §412.17 of the Neptune Township Land Development Ordinance, except in the case of residential development, which shall be governed by the parking requirements contained in the most recent edition of Residential Site Improvement Standards (R.S.I.S.) at NJAC 5:21.

The parking standards contained in the Township's Land Development Ordinance and R.S.I.S. may be reduced pursuant to site plan approval of a development within the Study Area in accordance with the provisions that follow.

Reductions in parking would apply where the applicant demonstrates that there are either:

- Variations in the accumulation of vehicles by hour, by day, or by season; or
- Relationships that exist among the land uses that result in visiting multiple land uses on the same auto trip; or
- Another shared parking approach.

Parking may be reduced at the discretion of the Planning Board or Zoning Board of Adjustment to the following requirements:

- Retail: One (1) space for each 500 square feet of gross floor area
- Apartments: 1.3 spaces per each one-bedroom unit. 1.8 spaces per each two-bedroom unit.
- Offices: One (1) space for each 300 square feet of gross floor area
- Restaurants, bars, and taverns: no parking is required

2. Buffering

All parking lots visible from public rights-of-way shall be screened by plantings, berms, and/or walls. These walls shall be no less than three (3) feet high and no more than four (4) feet high.

3. Signage

Signage within the Study Area shall be governed by the regulations contained in §416 of the Neptune Township Land Development Ordinance.

**APPENDIX A:
BACKGROUND STUDY AND
TECHNICAL REPORT**

APPENDIX

**BACKGROUND STUDY AND TECHNICAL REPORT
NEPTUNE TRANSIT VILLAGE STUDY AREA**

**TOWNSHIP OF NEPTUNE
MONMOUTH COUNTY, NEW JERSEY**

AUGUST 7, 2007

PREPARED FOR:

TOWNSHIP OF NEPTUNE

Prepared by:

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TABLE OF CONTENTS

LAND USE INVENTORY **1**
Introduction 1
Existing Land Uses 1
Occupancy, Tenure & Vacancy Status 2

TRANSPORTATION NETWORK **4**
Accessibility to Rail Transit 4
Roadway Network 4
Bus Service 5

LAND USE REGULATIONS **6**
Zoning: Districts & Regulations 6
Design Standards 7

MASTER PLAN CONSIDERATIONS **10**
Township of Neptune Master Plan 10
Master Plan Amendment and Neptune Strategic Redevelopment Plan 10
Rehabilitation Area 10

STATE, COUNTY, AND SURROUNDING MUNICIPAL PLANS **11**
State Plan 11
Monmouth County Plans 11
Adjacent Municipalities 12

OTHER PLANNING INITIATIVES **14**
Transit Village Initiative 14
Brownfields 15
Public Open Spaces 15

NEXT STEPS **16**

APPENDIX 1
Land Use: Blocks 162 through 165

APPENDIX 2
Use Regulations: LI (Light Industrial) zone

APPENDIX 3
Bulk Regulations: LI (Light Industrial) zone

APPENDIX 4
Transit Schedule- Train

APPENDIX 5
Photos

FIGURE 1 AERIAL OF STUDY AREA

FIGURE 2 EXISTING LAND USE

LAND USE INVENTORY

Introduction

The Neptune Transit Village Study Area (“Study Area”) includes a mix of land uses, among these being commercial and light industrial, retail sales and services, a restaurant, offices, auto-related uses and single family residences, mobile homes, and multifamily housing. Figure 1 is an aerial of the Study area and immediately adjacent properties. Figure 2 provides an illustration of the distribution of existing land uses within the Study Area.

The Study Area encompasses approximately 14.30 acres and is located within walking distance of the Bradley Beach Train Station. Building conditions and sizes are varied. Further information about the Study Area is provided in the following sections of this report.

Existing Land Uses

Table 1-1 summarizes the existing land uses in the Study Area as of 2007 by Property Tax Blocks. A detailed listing for each parcel is contained in Appendix 1.

Existing land uses were initially identified using the tax parcel mapping from the GIS of Monmouth County, which was then linked to the statewide MOD-IV tax assessor’s database. The data yielded by the MOD-IV database allowed a preliminary determination of existing land use, based on property tax assessment information.

This information was then verified by reviewing recent aerial photography and a field investigation undertaken on May 23, 2007.

TABLE 1-1A
Land Use by Tax Parcel in the Study Area
Township of Neptune, Monmouth County, NJ

Block	No. of Lots	1-Vacant	2-Resid’l.	4A-Commer.	4B-Indust.	4C-Aptmts.
162	19	12	6	0	0	1
163	18	0	0	4	14	0
164	25	0	11	6	7	1
165	21	5	1	5	10	0
Total	83	17	18	15	31	2

Source: MOD –IV Tax Assessor’s Database

TABLE 1-1B
Land Use by Area in the Study Area
Township of Neptune, Monmouth County, NJ

Block	Total Acreage	1-Vacant	2-Resid'l.	4A-Commer.	4B-Indust.	4C-Aptmts.
162	2.81	1.80	0.91	0.00	0.00	0.09
163	3.69	0.00	0.00	0.67	3.03	.000
164	3.79	0.00	1.49	0.90	1.27	0.12
165	4.01	1.25	0.05	1.01	1.70	0
Total	14.30	3.05	2.46	2.58	6.00	0.21

Source: MOD –IV Tax Assessor’s Database

Occupancy, Tenure & Vacancy Status

Table 1-2 details occupancy, tenure and vacancy status for Census Blocks 2006, 2007, 2009 and 2014 as of the 2000 US Census. ¹

TABLE 1-2
Occupancy, Tenure & Vacancy: 2000
Census Tract 8075, Block Group 2: Block(s) 2001, 2006, 2009, 2014
Township of Neptune, Monmouth County, NJ

Subject	Number	Percent
OCCUPANCY STATUS		
Total housing units	55	100.0%
Occupied housing units	46	83.6%
Vacant housing units	9	16.4%
TENURE		
Occupied housing units	46	100.0%
Owner-occupied housing units	25	54.3%
Renter-occupied housing units	21	45.7%
VACANCY STATUS		
Vacant housing units	9	100.0%
For rent	3	33.3%

¹ The Study Area comprises all of Blocks 2006, 2007 and 2009, and the majority of Block 2014, of US Census Tract 8075 Block Group 2.

TABLE 1-2
Occupancy, Tenure & Vacancy: 2000
Census Tract 8075, Block Group 2: Block(s) 2001, 2006, 2009, 2014
Township of Neptune, Monmouth County, NJ

Subject	Number	Percent
For sale only	0	0.0%
Rented or sold, not occupied	1	11.1%
For seasonal, recreational, or occasional use	5	55.6%
For migratory workers	0	0.0%
Other vacant	0	0.0%
Homeowner vacancy rate	N/A	0.0%
Rental vacancy rate	N/A	12.5%

N/A: Not applicable

SOURCE: US Bureau of Census 2000.

COMPILED BY: T&M ASSOCIATES

Of the 55 housing units as of 2000 in the study area, 46 units (or 83.6%) were occupied. Of the 46 occupied units, 25 (or 54.3 %) were owner-occupied and 21 (or 45.5 %) were renter-occupied.

Of the 55 housing units as of 2000, nine (9) units (or 16.4 percent) were vacant. One third of the nine (9) vacant units were rentals. The homeowner vacancy rate was 0.0 %, and the rental vacancy rate was 12.5 %.

Most residences are 1 or 2-story and there is a trailer park of about 16 units at Sixth Avenue and Atkins Avenue.

TRANSPORTATION NETWORK

Rail Schedule and Use along with Accessibility to the Train Station

There are five morning peak period trains headed north from 5:34 AM to 7:33 AM. The train ride is about 1 hour and 15 minutes to Newark with connections to Hoboken and New York City. In the afternoon peak period, there are eight trains out of New York City and Hoboken between 4:17 PM and 6:13 PM (see Appendix 4). According to NJ Transit, the 1999 Inbound AM Peak Period had 159 passengers with 206 inbound passengers all day. NJ Transit projects a 2025 AM Peak Period of 390 passengers. The adjacent stations in Belmar and Asbury Park have about the same 1999 ridership, but passenger growth at Bradley Beach is expected to increase more than at the other two stations. Weekday average boardings at the three stations for 2005 were Bradley Beach 224, Belmar 369, and Asbury Park 611.

Motor vehicle access to the station is available from driveways along Lareine Avenue (7th Avenue in Neptune Township), and Brinley Avenue (6th Avenue in Neptune Township). Approximately 75 parking spaces are available at the Bradley Beach Station. The parking spaces are reserved for transit users and available on a first-come, first-served basis. Four of these parking spaces are reserved for the concessionaire in the Station and another four are for American Disabilities Act purposes. The station does provide bicycle spaces.

There is a protected crossing pedestrian crossing along Memorial Drive at the traffic signal at Sixth Avenue and Brinley Avenue. Pedestrian crossing can be made at Seventh Avenue and Lareine Avenue; however, there is no signal protection along Memorial Drive. The entire study area is within a five-minute walk of the rail station.

Roadway Network

Memorial Drive (CR 40-A) parallels the railroad tracks as a four-lane, undivided roadway with no parking, no shoulders and a 30 mph speed limit. The north-south roadway is straight and level in this area with sidewalk along the western side of the road, isolated highway lighting at each intersections providing minimal pedestrian visibility, and a traffic signal at Memorial Drive and Sixth Avenue. The northbound approach has a NO TURN ON RED sign. The block between Sixth Avenue and Seventh Avenue has a sidewalk area along the eastern side of Memorial Avenue that serves the road and the station platform.

The other north-south route is the western boundary of the study area; Atkins Avenue located about 600 feet west of Memorial Drive. Atkins Avenue is a two-lane road with parallel parking along both sides, a 35 mph speed limit, and sideways along each side of the road. There are no traffic signals along this segment of Atkins Avenue, but there is one blinker at Seventh Avenue.

The cross streets of Fifth Avenue, Sixth Avenue (CR 2), Seventh Avenue, Eighth Avenue and Ninth Avenue are two-lane streets with parallel parking on each side and sidewalks on each side of the street. The speed limits are 25 mph on all streets except Sixth Avenue, which is 30 mph. Sixth Avenue extends through Neptune and Bradley Beach with a traffic signal at Memorial

Drive. Seventh Avenue goes across the railroad tracks into Bradley Beach, but Fifth Avenue, Eighth Avenue, and Ninth Avenue do not connect to other streets in Bradley Beach across the railroad tracks.

There are no separate facilities or delineated bike routes or lanes that are open to bicyclists only. Although some bicycles were parked near the train station, bicyclists must share the road and obey the rules of the road in traveling to the station or through the study area.

Bus Service

Neptune Township has no NJ Transit bus service directly serving this area. Along Route 71 (Main Street) in Bradley Beach, there are four active routes: the 317 going from Asbury Park to Fort Dix, and Philadelphia; the 830 Asbury Park to Point Pleasant Beach; the 832 from Red Bank and Monmouth Mall to Asbury Park; and the 837 from Long Branch to Seaview Square, Ocean Township. These buses can tie into the North Jersey Coast Line timetable at the Bradley Beach Station or other stations along the North Jersey Coast Line.

LAND USE REGULATIONS

Zoning: Districts & Regulations

Zoning Districts and underlying regulations are set forth in Article III (*Zoning Districts and Zoning Map*) and Article IV (*Zoning Districts Regulations*) of the Land Development Ordinance (LDO) of the Township of Neptune.

Performance Standards are set forth in Section 402 (*Performance Standards for All Uses*) of the Neptune LDO. In general these regulations cover the following: electrical and/or electronic devices, glare, heat, noise, odor, storage and waste disposal, ventilation and vibration.

Use and Bulk Regulations: the Study Area is located within the LI (Light Industrial) zone district of the Township, and allows for a wide range of non-residential uses; mobile home parks are a conditionally-permitted use. Appendix 1 and Appendix 2 (located at the end of this report) provides a comparison of the permitted principal uses and bulk regulations, respectively, of the LI zoning district.² An examination of Appendix 3 reveals that no parcels within the Study Area currently conform to the minimum lot size of 40,000 square feet.

Parking and Loading: Section 412.17 of the LDO specifies general parking requirements applicable to all districts in Neptune Township. Residential parking standards are those outlined in New Jersey's Residential Site Improvement Standards (RSIS). In addition, the following parking and loading regulations apply to current uses, or contemplated mixed used, in the Study Area:

- Section 411.04 (*Motor Vehicle Parking in Residential Areas*): regulations apply to vehicle parking on any lot used for residential purposes.
- Section 411.06 (*Parking of Trucks and Buses*): in general, no trucks or buses shall be regularly parked in any residential district.
- Section 412.12 (*Loading Requirements*) regulates loading requirements for commercial uses in all zones in Neptune Township.
- Section 412.18 (*Parking in Front Setback*) prohibits parking spaces in any front yard setback area.

Corner/Through Lots: regulations for such lot configurations are set forth in Section 412.05 (*Corner and Through Lots*) of the Neptune LDO. The LDO specifies that any portion of a lot with a street line is subject to the front yard setback requirements of the underlying zone. All lots are to have a rear yard, except in instances where a lot is bound by three (3) or more streets, in which case the property shall contain multiple front yards and at least one (1) side yard.

² Section 415.11 (Mobile Home Parks) of the Township of Neptune Land Development Ordinance ("LDO") contain additional requirements regarding minimum separation distances between mobile homes within a mobile home park.

Driveways: standards for driveways are set forth in Section 412.06 (*Driveways*) of the Neptune LDO. Of note is the fact that current regulations specify that non-residential driveways must be located no closer than within ten (10) feet of an existing adjacent residential property or within five (5) feet of any other property line.

Fences/Walls/Light Fixtures: are regulated by Section 412.07 (*Fences and Walls*) and Section 412.11 (*Light Fixture Mounting Height*), respectively. Of note is that fences and walls are considered structures and require an approved zoning permit prior to construction, and that lighting mounting height is limited to fourteen (14) feet or the height of the building, whichever is less.

Yards: specific requirements are outlined in Section 412.22 (*Yards*). As the Study Area is east of Route 35, the standard is that yard setback requirements within the LI zone east of Route 35 shall be fifty percent (50%) less than required setbacks in all other LI zones. This Section of the LDO also provides that buildings need not be set back from the street line any farther than existing buildings. Open space around a principal building (for the purposes of complying with the front, side, or rear yard requirements) cannot be utilized for the required yard areas of another principal building.

Signs: general provisions and design requirements for signs are set forth in Section 416 (*Signs*) of the LDO. A stated purpose of the sign regulations is “to encourage signs that promote a desirable visual environment through creative yet orderly design arrangements.”

Design Standards

Design standards are set forth in Article V (*Performance and Design Standards*) of the LDO. Many of these standards may not be appropriate for a Transit Village and could be revised under a form based code that will help articulate the guidelines of the Vision Plan.

The stated purposes of this section of the LDO include setting forth guidelines and standards that promote the creation of functional and attractive development, to ensure that any development shall comply with the Master Plan, zoning plan and other overall or district plans of the Township, to ensure that any new development gives due consideration to the physical, visual and spatial characteristics of the existing streetscape, neighborhood and district, and to minimize adverse impacts of flooding, drainage, erosion, vehicular traffic, pedestrian movement, and parking.

Exceptions/Waivers to Design Standards: it is noted in Section 501 (*Exceptions*) of the LDO that the design guidelines and standards are not intended to restrict creativity, and an applicant may request that the guidelines and standards be modified or waived.

Architectural Standards: Section 502 (*Architectural Design Standards*) generally regulates massing, horizontal courses of buildings, continuity of exterior treatments, roofs, windows, building entrances, physical plant (such as placement of HVAC units), materials used (including colors and details), shutters, lighting, signage, awnings and canopies. Of particular note are

design requirements applicable to multiple uses (buildings with multiple storefronts or other multiple uses), corner buildings or development plans that contain multiple buildings, and specific standards for retail, industrial and warehouse structures.

Landscaping/Screening: Landscaping and screening standards are set forth in Section 509 (*Landscaping*) and Section 503 (*Buffering and Screening*). Of note is the requirement to screen and buffer any non-residential uses from adjacent residential uses.

Driveway and Sight Triangles: in addition to the zoning requirements previously discussed in the Report, driveways are subject to design requirements set forth in Sections 505 (*Driveway Design Standards*) of the LDO. Of note is that every use shall have driveway access to a street, and that where a driveway approaches a street or right-of-way clear sight triangles must be provided. Additional requirements for sight triangles are set forth in Section 520 (*Sight Triangles*).

Curb and Sidewalk Design: curb design is regulated by Section 504 (*Curb Design Standards*) of the LDO. Of note is that the area located between curbs and sidewalks or walkways require planting strips. Non-residential sidewalk design requirements are set forth in Section 519 (*Sidewalks and Bikeways for Non-Residential Uses*). The form based design criteria may emphasize more specifics on the streetscape design.

Historic Preservation: any site of historic and architectural importance in the Township which has been designated by the Township or is listed by local, County, State or national agencies are subject to Section 508 (*Historic Preservation Design Guidelines*) of the Neptune LDO.

Parking/Loading and Lighting: in addition to the zoning requirements previously discussed in the Report, parking and lighting are also subject to design standards set forth in Section 514 (*Parking Lot and Loading Area Design Standards*) and Section 511 (*Lighting*), respectively, of the Neptune LDO. In regards to parking, the design requirements specify that parking is not allowed in a required front yard and a parking lot shall be located to reduce visual impact. Development proposals should provide sufficient lighting on each site or along roadways to ensure the security of property and to protect the safety of persons between the hours of sunset and sunrise when the establishment or facility is in use.

Open Space: as per Section 513 (*Open Space*), developed open space shall be designed to provide active recreational facilities in accordance with National Recreation Association standards to serve the residents of the development; undeveloped open space shall be designed to preserve important site amenities and environmentally sensitive areas. Common open space shall remain in private ownership, unless the appropriate Board of jurisdiction determines that public ownership is desirable.

Refuse & Recycling: as per Section 515 (*Refuse & Recycling Areas*), all uses must provide an area used for collection of refuse and recycling materials; all containers, bins, dumpsters and/or storage facilities shall be designed to reduce discernible odors and contain such within the storage facility area.

Residential Development: design regulations for residences are set forth in Section 516 (*Residential Development*). Layouts of multi-family residential buildings (and units within) are specified. As per Section 517 (*Residential Site Improvement Standards*), all site development proposals are subject to RSIS standards.

Security Grilles / Roll-Up Doors: are permitted for non-residential uses only as per Section 518 (*Security Grilles and Roll-Up Doors*).

Site Plan and Subdivision Design Standards: Section 521 (*Site Plan Development Standards*) and Section 524 (*Subdivision Development Standards*) set forth the standards for site plan and subdivision proposals, respectively. Applicable to future development in the Study area is the site plan requirement that buildings shall be located to front towards and relate to a public street; for multiple-building developments buildings located on the interior of a site shall front towards and relate to one another. As no lots in the Study Area conform to minimum lot requirements, the subdivision requirement that all resulting lots shall have dimensions consistent with the requirements of the zoning district in which they are located is pertinent.

Trees: are regulated in regards to placement (Section 523: *Street Trees*) and removal (Section 525: *Tree Removal and Tree Cutting*). Street trees shall be installed on both sides of all public and private streets in accordance with an approved landscape plan. Up to fifty (50%) percent of the trees may be removed (without replacement) for the sole purpose of clearing for the building envelope, driveways and yard space only, for new residential developments. All nonresidential developments are required to replace all trees removed in accordance with the LDO.

Utilities: as per Section 526 (*Underground Wiring*), all utilities (electric, telephone, television and other communication facilities) servicing new developments shall be provided by underground wiring within easements and dedicated public rights-of-way.

Retaining Walls: are subject to design and maintenance standards as set forth in Section 527 (*Retaining Walls*) of the Neptune LDO.

A form based code for a Transit Village zone will need to revise many of these standards and will provide visual representations to expedite development approval in the transit village area.

MASTER PLAN CONSIDERATIONS

Township of Neptune Master Plan

The Master Plan of the Township of Neptune was prepared in 2000 and amended in May 2002, October 2004 and December 2005. The Study Area was included in the Light Industrial Land Use Category; such areas were recognized as being most suitable for light manufacturing activities. The Master Plan recognized two (2) existing mobile home parks in the Light Industrial Area, and stated that appropriate conditional use standards should be provided. The Master Plan noted (in relation to the mobile home parks) that “generally, residential land uses are incompatible with neighboring industrial uses.”

Master Plan Amendments and the Neptune Strategic Revitalization Plan

Gateways to Neptune: A Strategic Revitalization Plan (NSRP) was adopted by the Neptune Planning Board on October 13, 2004 as an amendment to the Township’s Master Plan. One of the Core Concepts of the Plan was the goal of creating a “transit village” in the Study Area, a Bradley Park neighborhood next to the Bradley Beach Train Station. The NSRP proposed to take advantage of the location of the Study Area to create a compact mixed use community within walking distance of the Bradley Beach train station.

The redevelopment of the Study Area into a mixed-use “Transit Village” has been identified as a high priority and one of the “core concepts.” The Plan is consistent with Smart Growth principles. This effort will develop a Vision Plan with the community that can be used to shape the future development in the Transit Village Area.

Rehabilitation Area

It is also noted that the Township has designated the Study Area as an area in need of rehabilitation pursuant to the Local Redevelopment and Housing Law (LRHL; NJSA 40A:12A-1). This designation permits the Township to provide five-year tax abatements for improvements to residential and commercial properties. In addition, the Township could adopt a redevelopment plan for the area. However, the Township cannot use eminent domain for redevelopment purposes in a rehabilitation area.

STATE, COUNTY, AND SURROUNDING MUNICIPAL PLANS

State Plan

State Development and Redevelopment Plan: The State Planning Commission adopted the State Development and Redevelopment Plan (SDRP) in June of 1992 and adopted a revised SDRP on March 1, 2001. The SDRP contains goals, objectives, and policies regarding the future development and redevelopment of New Jersey. The primary objective of the SDRP is to guide development to areas where infrastructure is available or can be readily extended such as along existing transportation corridors, in developed or developing suburbs, and in urban areas. New growth and development should be located in “centers,” which are “compact” forms of development, rather than in “sprawl” development. The overall goal of the SDRP is to promote development and redevelopment that will consume less land, deplete fewer natural resources and use the State’s infrastructure more efficiently. Among these is the redevelopment and revitalization of New Jersey’s cities and urban areas.

The SDRP places Neptune Township within the Metropolitan (PA1) Planning Area. As set forth in the 2001 SDRP:

Metropolitan Planning Area: PA1 Provide for much of the state’s future redevelopment; revitalize cities and towns; promote growth in compact forms; stabilize older suburbs; redesign areas of sprawl; and protect the character of existing stable communities.

This Planning Area includes a variety of communities that range from large Urban Centers such as Newark, to 19th century towns shaped by commuter rail and post-war suburbs, such as Englewood and Cherry Hill. ... This Planning Area can also be found among the older shore towns of Monmouth County, Atlantic County, along the Delaware River in Salem County, and in the Bridgeton and Vineland-Millville areas in Cumberland County (underline added).

A new SDRP was released for cross acceptance in 2004. The Monmouth County Cross Acceptance Report was submitted in November, 2004. The Comparison Phase of the Cross Acceptance process has ended and the Negotiation Phase is currently underway.

Any development efforts made within the Study Area should be in accordance with the SDRP’s stated intents for areas designated PA 1.

Monmouth County Plans

1. Monmouth County Growth Management Guide: Monmouth County has been divided into 5 regions. The 1995 Growth Management Guide continues to serve as the County Master Plan for regions where plans have yet to be adopted. A plan for the Coastal region, of which Neptune Township is a part, is currently being prepared and has a target date of March 2008 for adoption.

Any development efforts made within the Study Area should be in accordance with the Growth Management Guide and should take into account the Coastal Plan upon its adoption. County approvals will have to be obtained for any proposed changes to Memorial Drive, which is a County Road.

2. Coastal Monmouth Plan: Monmouth County is developing a Long Range Plan as a Smart Growth Study with 30 Shore municipalities to look at regional issues and solutions. Neptune Township is one of the participants as part of the South Central Region (Avon to Deal), and coordination of the Township's input benefits the region. The plan will be released in draft form in 2008.
3. Monmouth County Open Space Plan and Monmouth County Scenic Roadway Plan: These two countywide plans did not identify any specific actions that will impact the study area.

Adjacent Municipalities

1. Bradley Beach Master Plan: The NJ Transit Train Station located east of the adjacent to the Study Area lies within the borders of Bradley Beach Borough. The NJ Transit Right of Way forms part of Neptune Township's municipal border with Bradley Beach. The Borough's Master Plan was adopted in 1988.

The Borough has zoned the area around the Train Station as General Business, with areas north of the Train Station zoned a Business, Office and Retail (B-O-R) zone. Both zones allow various non-residential uses, and Townhouses are a permitted Conditional Use.

The 1988 Master Plan recommended that a Special Improvement District (SED) be created for areas parallel to the ROW. Under certain conditions of more intensive development would be allowed for this area. A major condition specifically stated is a developer would have to acquire an entire block. The Master Plan stated that the use of block development would avoid "haphazard piecemeal development" in the SED.

The Master Plan Re-Examination Report (revised June 5, 1996) notes that Bradley Beach was developing a Special Improvement District for the Main Street Business District.

Any development efforts made within the Study Area take into account the designated land uses for the adjacent Bradley Beach area.

2. Neptune City Master Plan: The southern border of the Study Area borders Neptune City Borough. The Borough's Master Plan was adopted in 1999.

The Borough has zoned the area adjacent to the Study Area as Highway Commercial (HC). A variety of non-residential and residential uses are permitted.

The 1999 Master Plan recommended Block 1, immediately to the south of the Study Area, keep the HC zoning classification for frontage along Fifth Avenue, with the vacant portion of Block recommended to be zoned residential. This vacant portion of Block 1 now contains a recently built condominium development, consisting of four (4) separate buildings.

Any development efforts made within the Study Area take into account the designated land uses for the adjacent Neptune City area.

OTHER PLANNING INITIATIVES

Transit Village Initiative

The Transit Village Initiative, headed by NJ TRANSIT and the New Jersey Department of Transportation (NJDOT), aims to help redevelop and revitalize communities around transit facilities by making them an appealing choice for people to live, work and play, thereby reducing reliance on the automobile.

The five (5) essential criteria for Transit Village designation are as follows:

1. Willing to grow in jobs, housing and population near transit;
2. Existence of a transit facility;
3. Opportunities for redevelopment near transit, such as vacant land, underutilized or deteriorated buildings, brownfields or grayfields;
4. Adopted zoning and/or redevelopment plan based on transit-oriented development (T.O.D.) principles; and
5. Increase in residential development.

The intent of the Vision Plan will be to develop a vision for the community that defines in more detail the means to fulfill these five essential criteria.

Additionally, the TOD practice encourages the area to:

1. Have “ready-to-go” projects.
2. Demonstrate pedestrian and bicycle friendliness.
3. Use transit station as a focal point of the community.
4. Consider the Transit station part of a station area management plan, a SID, or Main Street NJ.
5. Minimize auto use by maximizing the appeal of transit.
6. Provide commuter parking for residents and non-residents & implement shared parking solutions.
7. Support local arts and culture.
8. Support historic and architectural integrity of the area architectural design guidelines- facades, window replacements, awnings, lighting and signs.
9. Consider how to incorporate affordable housing into the residential component.

These additional features will be considered and discussed with the stakeholders to determine a transit village mix that is acceptable to the Township and the whole community with possible joint support from Bradley Beach.

Brownfields

New Jersey Law defines brownfields as “any former or current commercial or industrial site that is currently vacant or underutilized and on which there has been, or there is suspected to have been, a discharge of a contaminant” (NJSA 58:10B-23.d).

The State has established the New Jersey Brownfields Redevelopment Task Force, which assists municipalities and counties in the local implementation of Smart Growth strategies through brownfield redevelopment. In addition, brownfield redevelopment is specified by the Transit Village Initiative as an opportunity for redevelopment.

The only site listed on the NJDEP “known contamination sites” in the study area is the Adams tract listed at 509 Memorial Drive. The remedial level of the property is C2 (remediation requires a formal design). C2 sites such as the Adams tract are properties where the source of the contamination is known or the release has caused ground water contamination. There may be additional sites that have not been tested or identified at this time.

Public Open Spaces

Public Open Spaces are an important part of any community’s civic and commercial center. They draw residents and visitors to the area by providing enjoyable places to congregate and relax. The Study Area does not currently have such an area.

The Study Area may benefit from additional public and/or open spaces. Public and/or open space could be provided in the eastern sector of the Study Area, toward the Bradley Beach Train Station. Such spaces could range from seating alcoves to public parks or plazas.

NEXT STEPS

The Township will share the preceding data collection and background information with the community, stakeholders, and professionals to discuss what changes could help expedite development of a Transit Village consistent with the Neptune Strategic Redevelopment Plan objective. The stakeholders will be given a survey to help determine the direction of growth that is desired by the community.

Examples of other Transit Villages will be distributed to the Township and stakeholders for their consideration. The elements of the Transit Village plan will include a design for the streets and circulation to and from the station, identify general massing of buildings and locate the various preferred uses and open spaces. These will be illustrated and described as a form based code that facilitates the development approval process.

Based on an examination and discussion of the potential changes, a conceptual transit village plan will be developed for the study area along with a vision plan. The vision plan will identify potential future development levels along with connectivity and design principles to facilitate improved access to the train station. These concepts will be laid out on a key design concepts map.

As part of the means to achieve the vision plan, the Master Plan may need to be amended and the Local Development Ordinance will have to be amended. Discussions with Neptune Township will determine whether the proposed form-based code is an overlay zone, replacement of the existing zoning, or a new Transit Village. The objective will be the adoption of a form based design standard based on a consensus-driven vision plan that will guide developers, and property owners to understand the types and forms of development that are acceptable to Neptune Township in this transit village area.

APPENDIX 1

Land Uses: Blocks 162 through 165

Township of Neptune, Monmouth County, NJ

Block	Lot	Additional Lots	Property Owner	Use Classification		Acreage	Year Built	Percentage of Total	
				MOD -IV	Field Investigation			Block	Study Area
162	1		STEPNEY	2	Single Family	0.1157	1934	4.12%	0.81%
162	2		MATARAZZO	2	Single Family	0.1618	1950	5.76%	1.13%
162	9		RUSSO	2	Single Family	0.1515	1934	5.39%	1.06%
162	10		TAYLOR	2	Single Family	0.1584	1934	5.64%	1.11%
162	11		PARRISH	2	Single Family	0.1429 ①	1934	5.09%	1.00%
	①	12	PARRISH	2	Vacant unimproved	0.1429 ①	N/A	5.09%	1.00%
162	67	68,69,77-79	J.J.P.P. B – 162	1	Vacant unimproved	0.8609	N/A	30.65%	6.02%
162	70	71	ROSELLE/ CORSO	2	Vacant unimproved	0.365 ②	N/A	12.99%	2.55%
	②	72	ROSELLE/ CORSO	2	Single Family	0.1825 ②	1934	6.50%	1.28%
162	73		CAMPBELL	4C	Apt. Building	0.0934	1950	3.33%	0.65%
162	74	75,76	J.J.P.P. B – 162	1	Vacant unimproved	0.4339	N/A	15.45%	3.03%
Block Total						2.8089		100.00%	19.65%
163	53	54-59	ADAMS	4B	Industrial	1.14	1906	30.88%	7.97%
163	60	61 & 67	J.J.P.P. B – 163	4B	Industrial	0.291	X	7.88%	2.04%
163	62	63-66	J.J.P.P. B – 163	4B	Industrial ③	0.75	1960	20.31%	5.25%
163	69		J.J.P.P. B – 163	4B	Industrial ④	0.4454	1946	12.06%	3.12%
163	70		KAWUT	4B	Industrial ④	0.4	1950	10.83%	2.80%
163	71		SEVENTH AVENUE, LLC	4A	Commercial (appears vacant)	0.287	1940	7.77%	2.01%
163	72	73	SAUTA	4A	Auto - related	0.21	1960	5.69%	1.47%
163	74		SAUTA	4A	Auto - related	0.1687	1950	4.57%	1.18%
Block Total						3.6921		100.00%	25.82%
164	1		ABRAMS	4A	Auto - related	0.1079	1958	2.85%	0.75%
164	2		MENNIE	4C	Apt. Building	0.1217	1934	3.21%	0.85%
164	3		PEREZ, D	2	Single Family	0.1079	1931	2.85%	0.75%
164	4		MACKEY #1	2	Single Family	0.1584	1934	4.18%	1.11%
164	5		SANTIAGO	2	Single Family	0.1894	1935	5.00%	1.32%

APPENDIX 1

Land Uses: Blocks 162 through 165

Township of Neptune, Monmouth County, NJ

Block	Lot	Additional Lots	Property Owner	Use Classification		Acreage	Year Built	Percentage of Total	
				MOD -IV	Field Investigation			Block	Study Area
164	6		MCCUDDEN	2	Single Family	0.1722	1935	4.55%	1.20%
164	7		BOWNE	2	Single Family	0.124	1940	3.28%	0.87%
164	8		KELLY	2	Single Family	0.124	1940	3.28%	0.87%
164	9		PEREZ, M	2	Single Family	0.124	1940	3.28%	0.87%
164	10		MICHEL	2	Single Family	0.1171	1935	3.09%	0.82%
164	37		FORTE	4B	Retail Sales & Services	0.1406	1950	3.71%	0.98%
164	38	39	PRIDE PROPERTIES	4B	Industrial	0.287	1964	7.58%	2.01%
164	40		VORWERK	4A	Auto - related	0.27	1938	7.13%	1.89%
164	41		KIERAN	4A	Retail Sales & Services	0.093	1950	2.46%	0.65%
164	42		DAY	4B	Retail Sales & Services	0.3444	1940	9.10%	2.41%
164	44		PEDERSON	2	Single Family	0.1687	1950	4.46%	1.18%
164	45	46	JAMES	2	Single Family	0.2032	1935	5.37%	1.42%
164	47		GERIN CORP.	4B	Office	0.1722	1940	4.55%	1.20%
164	48	49	MACKEY #2	4B	Retail Sales & Services	0.3271	1941	8.64%	2.29%
164	50	51,52	PRIDE PROPERTIES	4A	Retail Sales & Services	0.4333	1960	11.44%	3.03%
Block Total						3.7861		100.00%	26.48%
165	13	14,29-36	1100 6TH AVENUE, L.L.C.	4B	Industrial ☺	1.7	1946	42.39%	11.89%
165	16	17-18	SCHILLER	1	Vacant	0.52682 4 ☺	X	13.14%	3.68%
165		19 ☺	SCHILLER	1	Vacant unimproved	0.17217 6 ☺	N/A	4.29%	1.20%
165	20		SCHILLER	1	Vacant unimproved	0.5479	N/A	13.66%	3.83%
165	23		ALDARELLI REALTY	4A	Restaurant	0.29	1940	7.23%	2.03%
165	24.01		ALDARELLI REALTY	2	Single Family	0.0539	1940	1.34%	0.38%
165	25	26-28	METPARK II	4A	Trailer Park / Mixed Use	0.72	1930	17.95%	5.04%
Block Total						4.0108		100.00%	28.05%
Study Area Total						14.2979		N/A	100.00%
Median Age: All Structures:							1940		

APPENDIX 1

Land Uses: Blocks 162 through 165

Township of Neptune, Monmouth County, NJ

Block	Lot	Additional Lots	Property Owner	Use Classification			Year Built	Percentage of Total	
				MOD -IV	Field Investigation	Acreage		Block	Study Area
Median Age: Residential Structures							1940		
Median Age: Non – Residential Structures							1946		
MOD IV Classifications: 1: Vacant Land; 2 Residential (four families or less); 4A Commercial; 4B Industrial; 4C Apartment									
① MOD IV data for Block 162 Lot 11 (additional Lot 12) split for this Table due to vacant land condition for Lot 12.									
② MOD IV data for Block 162 Lot 70 (additional Lots 71 & 72) split for this Table due to vacant land condition for Lots 70 and 71									
③ Block 163 Lots 64 through 66 = industrial use with bay door facing street.									
③ Block 163 Lots 64 through 66 = industrial use with bay door facing street.									
④ Industrial use with bay door facing street.									
⑤ Block 165 Lots 29 through 31 = industrial use with bay door facing street.									
⑥ MOD IV data for Block 165 Lot 16 (additional Lots 17 through 19) split for this Table due to vacant land condition for Lot 19									
N/A: Not applicable									
X: Not available									

SOURCE: NJ Division of Taxation MOD IV assessment administration system (May 2, 2007).

COMPILED BY: T&M ASSOCIATES

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Residential		Mobile home park	
Development Options	Planned Unit Development		
Retail Trade	Christmas tree sales (outdoor); Heating/fuel oil dealers & distributors	Floor covering stores; Home furnishings stores; Household appliance stores; Radio, television & other electronics stores; Computer & software stores; Musical instrument Stores; Music stores (CDs, cassettes, videos, records); Building material and plumbing supply dealers; Power equipment stores; Cosmetics, beauty supplies & perfume store; Food (health) supplement stores; Health & personal care stores; Family clothing & apparel stores; Lingerie and corset stores; Swimwear stores; Jewelry stores; Shoe stores; Hobby, toy & game stores; Bicycle stores; Sporting goods stores (excluding firearms); Arts & craft shops; General merchandise stores; Office equipment, supplies & stationery stores; Pet food & pet supplies stores; Electronic shopping & mail-order houses	

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Finance & insurance	Commercial banking; Savings institutions; Bank; Credit unions; Consumer lending; Real estate credit; All other non-depository credit intermediation; Financial clearinghouse & reserve activities; Investment banking & securities dealing; Securities brokerage; Direct life Insurance carriers; Direct health & medical insurance carriers; Insurance agencies & brokerages; Claims adjusting; All other insurance related activities		
Real estate & rental & leasing	Offices of real estate agents; Offices of real estate appraisers		
Educational Services	Computer training facility; Apprenticeship training; Other technical & trade schools; Sports & recreation instruction; Art, music, dance & martial-arts instruction; All other miscellaneous schools & instruction		Educational support services

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Health care and social assistance	Offices of physicians (exc. mental health); Offices of physicians, mental health; Offices of dentists; Offices of chiropractors; Offices of optometrists; Offices of physical, occupational and speech therapists, and audiologists; Offices of podiatrists; All other outpatient care centers; Diagnostic Imaging centers; Home health care services; Ambulance and first aid services; Nonresidential services for elderly & disabled persons, including adult day care; Child Care Center		
Arts, entertainment, recreation & cultural	Health and Fitness Club; Indoor recreational facility; Public Community Centers; Public Parks and recreational facilities; Grantmaking foundations Civic & social organizations; Professional organizations; Labor unions & similar labor organizations		Recreational clubhouse, concession stand
Accommodation & food services	Hotels (excluding casino hotels); Hotels, Extended Stay Lodging Facilities; Full-service restaurants; Restaurant; Snack & nonalcoholic beverage bars	Restaurant, drive-through	
Personal Services			

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Retail services	General automotive repair & maintenance (mechanical & electrical systems); Automotive body repair, painting & interior repair & maintenance; Automotive oil change & lubrication shops; Installation of auto phones; Automotive Body Repair and Painting; Automotive Service Station; Welding shops and blacksmiths; Machine shops		
Professional services	Offices of lawyers; Offices of certified public accountants; Other accounting services; Architectural services; Landscape architectural services; Professional planning services; Engineering services; Building inspection services; Testing laboratories; Custom computer programming services; Computer systems design services; Computer facilities management services; Other computer related services; Administration & general management consulting services; Other scientific & technical consulting services; Advertising agencies; Other services related to advertising; Veterinary services / animal hospital; Photography studios & videography services; All other professional, scientific & technical services		

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Support services	Corporate, subsidiary & regional managing office; Office administrative services; Facilities support services; Employment placement agencies; Private mail centers; Quick printing; All other business support services; Travel agencies; Investigation and security services; Security systems services (except locksmiths); Janitorial services; Landscaping maintenance services; Carpet & upholstery cleaning services; Other services to buildings & dwellings; Municipal facilities; Fire department facilities; Public safety facilities; First aid facilities; Board of Education facilities; Public works facilities		
Resource Production			
Construction Trades	Plumbing, heating & AC contractors; Painting & wall covering contractors; Electrical contractors; Masonry & stone contractors; Carpentry contractors; Floor laying & other floor contractors; Roofing, siding, & sheet metal contractors; Concrete contractors; Structural steel erection contractors; Glass & glazing contractors; Excavation contractors; Landscaping contractors; Building equipment & other machine installation contractors; All other special trade contractors		

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Manufacturing	Frozen specialty food manufacturing; Animal slaughtering; Meat processed from carcasses; Rendering & meat by-product processing; Commercial wholesale bakeries; Knit fabric & lace mills; Canvas & related product mills; All other miscellaneous textile product mill; Lumber millwork (including flooring); Commercial lithographic printing; Other commercial printing; Prepress services; Medicinal & botanical manufacturing; Research Laboratory; Pharmaceutical preparation manufacturing; All other miscellaneous chemical product & prep manufacturing; Fabricated structural metal manufacturing; Sheet metal work manufacturing; Electroplating, polish, anodize, color; Industrial mold manufacturing; Other metalworking machinery manufacturing; Radio, TV broadcast & wireless communications equipment manufacturing; Other electronic component manufacturing; Automatic environmental control manufacturing; Surgical & medical instrument manufacturing; Dental laboratories; Sign manufacturing; All other miscellaneous light manufacturing		

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Wholesale trade	Automobile & other motor vehicle wholesale; Furniture wholesale; Home furnishing wholesale; Computer & peripheral equip & software wholesale; Other professional equipment & supplies wholesale; Metal service centers & offices wholesale; Electrical equipment, wiring supplies, construction materials wholesale; Other electronic parts & equipment wholesale; Hardware wholesale; Refrigeration equipment & supplies wholesale; Industrial machinery & equipment wholesale; Industrial supplies wholesale; Service establishment equipment & supplies wholesale; Recyclable material wholesale; Other miscellaneous durable goods wholesale; Stationery & office supplies wholesale; Industrial & personal service paper wholesale; Drugs & druggists' sundries wholesale; Women's, children's, infants' clothing wholesale; General line grocery wholesale; Packaged frozen food wholesale; shapes wholesale; Flower, nursery stock, florists' supplies; Tobacco & tobacco product wholesale; Other miscellaneous nondurable goods wholesale		

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Wholesale trade (continued)	Dairy product (excluding dried or canned) wholesale; Poultry & poultry product wholesale; Fish & seafood wholesale; Meat & meat product wholesale; Fresh fruit & vegetable wholesale; Other grocery & related products wholesale; Plastics materials & basic forms		
Transportation, storage & Warehousing	General freight trucking, long-distance (Truckload); General freight trucking, long-distance (Less Than Truckload LTL); Long term storage of wholesale goods; Flex storage space; Household & office goods moving; Business & other motor vehicle transit systems; Taxi service; School & employee bus transportation operations; Motor vehicle towing operations; Freight transportation arrangement; Couriers; Local messengers & local delivery	Self-storage facility	Helistops; Parking for principal use
Information	Wireless telecommunications facility; Newspaper publishers; Book publishers; Software publishers; Film & sound recording studios; Television & radio antenna broadcasting; Data Center	Billboard; Centralized telephone utility installations; Centralized coaxial cable utility installations; Wireless telecommunications facility	Telephone communication distribution; Coaxial cable communication distribution; Emergency services radio communication facilities

Appendix 2
Use Regulations: LI (Light Industrial) zone
Township of Neptune

Use Type:	Principal Permitted Uses	Permitted Conditional Uses	Permitted Accessory Uses
Utilities		Public Utility Facility; Electricity regulating substations	Natural gas distribution; Electric power transmission & distribution; Electric power generation plant; Public water supply purveyance & distribution; Sanitary sewer lines

SOURCE: Zoning Schedule A-2, Township of Neptune Land Development Ordinance

COMPILED BY: T&M ASSOCIATES

Appendix 3
Bulk Regulations: LI (Light Industrial) zone ³
Township of Neptune

Description	Requirement
Minimum Lot Area (square feet)	40,000
Maximum Density (d.u. per acre)	N/A
Maximum Floor Area Ratio	2
Minimum Lot Width (feet)	150
Minimum Lot Frontage (feet)	150
Minimum Lot Depth (feet)	200
Front Yard Setback (feet)	40
Side Yard Setback (feet)	25
Combined Side Yard Setback (feet)	50
Rear Yard Setback (feet)	25
Maximum Percent Building Cover	0
Maximum Percent Total Lot Cover	1
Maximum Number of Stories	4
Maximum Building Height (feet)	60
Minimum Improvable Area (M.I.A. in square feet)	16,800
M.I.A. - Diameter of Circle (feet)	84

SOURCE: Zoning Schedule B - Township of Neptune Land Development Ordinance

COMPILED BY: T&M ASSOCIATES

³ As per Section 412.22.D (Yards) of the Neptune LDO yard setback requirements within the LI zone east of Route 35 shall be fifty percent (50%) less than required setbacks in all other LI zones.

APPENDIX 4

Transit schedule- Train

APPENDIX 4

NORTH JERSEY COAST LINE

BRADLEY BEACH/NEPTUNE
MONDAY- FRIDAY
PEAK PERIOD TRAIN SCHEDULE

MORNING PEAK PERIOD TRAINS- AM						
Train No.	Bradley Beach	Newark	Hoboken	NYC- World Fin. Ctr.	NYC-Penn Stn.	
2300	5:24	6:44	7:05	7:13	7:13	7:13
2302	5:49	7:05	7:28	7:33	7:33	7:34
2304	6:26	7:46	8:08	8:18	8:18	8:18
2306	7:05	8:21	8:51	8:51	8:51	8:54
2308	7:33	8:51	9:16	9:23	9:23	9:19
AFTERNOON PEAK PERIOD TRAINS- PM						
	NYC-Penn Stn.	NYC- World Fin. Ctr.	Hoboken	Newark		Bradley Beach
2305	4:07	4:12	4:20	4:42	4:42	5:59
2307	4:41	4:39	4:48	5:09	5:09	6:26
3263+	5:03	4:51	XXXXXX	5:20	5:20	6:46
2309	5:03	5:01	5:13	5:32	5:32	6:46
3267+	5:23	5:15	XXXXXX	5:45	5:45	7:05
2311	XXXXXXXXXX	5:39	5:46	6:09	6:09	7:25
3271+	5:52	5:43	XXXXXX	6:13	6:13	7:56
2313	6:13	6:07	6:19	6:38	6:38	7:56
NYC-World Fin. Ctr. Via PATH						
NYC-Penn Station Via Newark Connection						

APPENDIX 5

PHOTOS

PHOTOS SHEET 1

Three views of Memorial Drive

Restaurant

PHOTOS SHEET 2

Residences

PHOTOS SHEET 3

Apartments

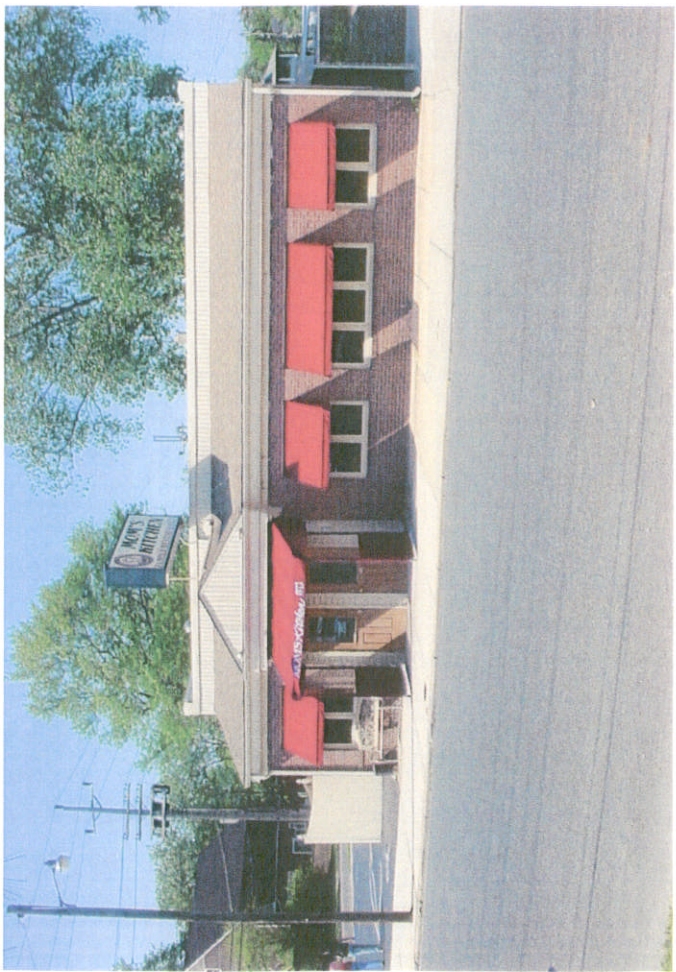
Business

Vacant Land

Underutilized Building

PHOTOS SHEET 4

Businesses













 11 Tindall Road
 Middletown, NJ 07748-2792
 Phone: 732-671-6400
 Fax: 732-671-7365
 ASSOCIATES
 0 62.5 125 250 Feet




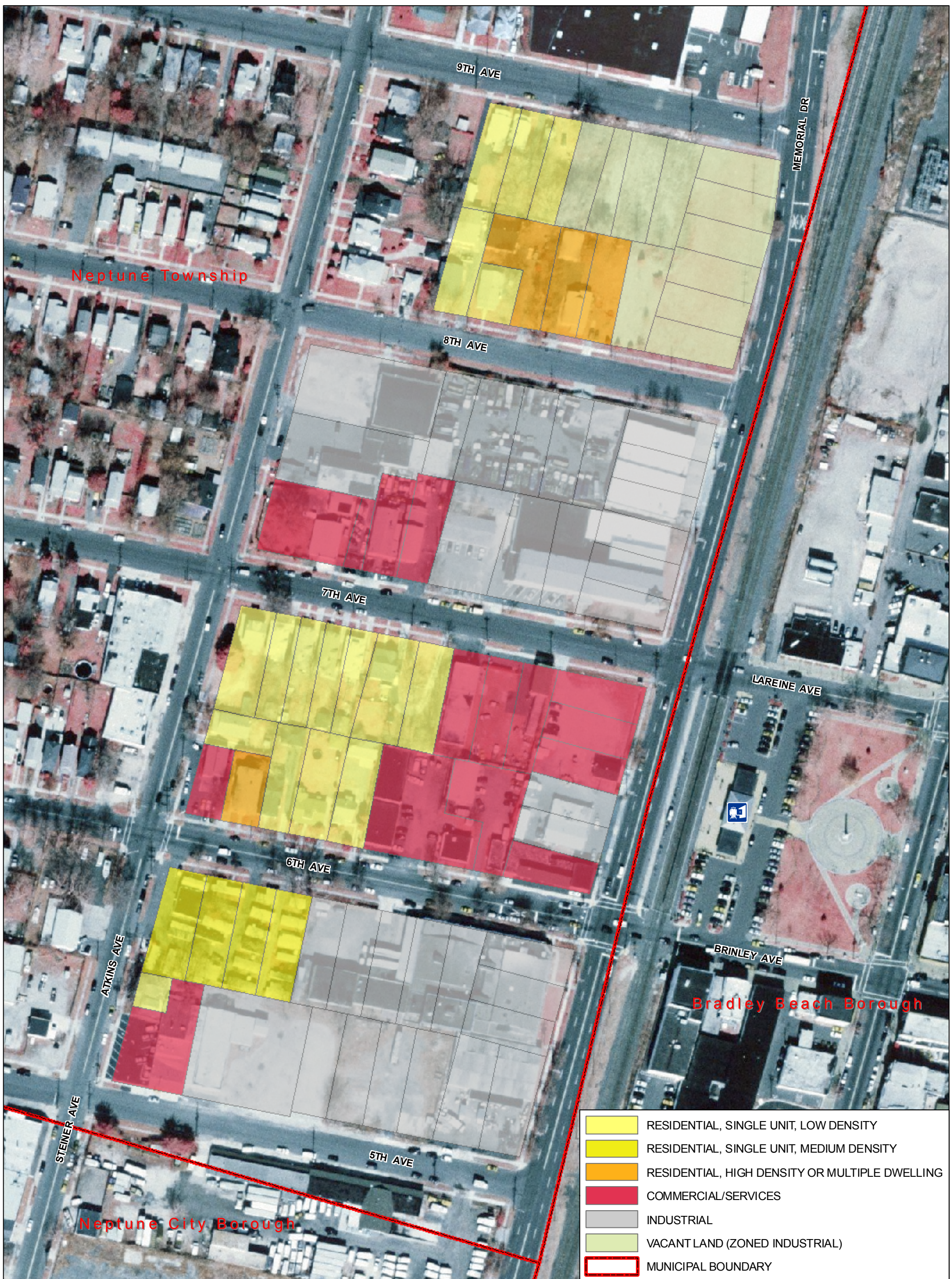
-  Study Area
-  Tax Parcels
-  Municipal Boundary

Figure 1: Aerial of Study Area
Neptune Township
Monmouth County, New Jersey

Prepared by: STK, August 7, 2007
 Source: NJDEP - 2002 Orthophoto Aerials, Municipal Boundary; Monmouth County GIS Dept. - Roads; Neptune Township Tax Maps - Tax Parcels, Study Area
 File Path: H:\NEPT\00130\GIS\Projects\nept130_fig1StudyArea_TABLOID.mxd



NOTE: This map was developed using New Jersey Department of Environmental Protection Geographic Information System digital data, but this secondary product has not been verified by NJDEP and is not State-authorized.



**Figure 2: Existing Land Uses
Neptune Township
Monmouth County, New Jersey**

T&M 11 Tindall Road
Middletown, NJ 07748-2792
Phone: 732-671-6400
FAX: 732-671-7365
ASSOCIATES
0 62.5 125 250
Feet

Prepared by: STK, August 7, 2007
Source: NJDEP - 2002 Orthophoto Aerials, Municipal Boundary;
Monmouth County GIS Dept. - Roads; T&M Field Survey - Land Use
File Path: H:\NEPT\00130\GIS\Projects\nept130_fig2Landuse_TABLOID.mxd



NOTE: This map was developed using New Jersey Department of Environmental Protection Geographic Information System digital data, but this secondary product has not been verified by NJDEP and is not State-authorized.